

MAJOR NODE BLOCK 360

BRONTE STREET S. & BRITANNIA RD.

URBAN DESIGN BRIEF & ARCHITECTURAL CONTROL GUIDELINES

TOWN OF MILTON



Prepared by:



Project No.: W-1917'D'

Prepared For:
Shearling Heights Estates Ltd.
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PART 1

DESIGN VISION,
GUIDING PRINCIPLES
AND OBJECTIVES



1.1 INTRODUCTION

This report has been revised from the previous UDB dated June 28, 2024, to reflect modifications made to the proposed Site Plan Concept for the subject lands.

This Urban Design Brief & Architectural Control Guidelines (UDB & ACG) has been prepared in accordance with the Town of Milton's "Urban Design Brief - Terms of Reference" and "Mid-Rise Guidelines" on behalf of Shearling Heights Estates Ltd., for their proposed development of the subject lands, Block 360 within the Main Sail Estates Subdivision (24T-14006/M | Registered Plan Number 20M-1184). The subject lands occupy an area of 21,103m² (5.2acres) located at the north-east corner of Bronte Street South and Britannia Road within the Town of Milton. The site is designated "Major Node Area" within the Boyne Survey Secondary Plan.

The purpose of the UDB & ACG is to demonstrate how the developer's proposed development plan supports the vision, goals and objectives established in the Town of Milton Official Plan, Secondary plan, Boyne Survey Urban Design Guidelines, Mid-rise Guidelines and other policies and studies. Furthermore, the document provides design guidance for the creation of a high quality, intensified, urban mixed-use built form situated at this prominent location within the Town of Milton. The intent is to ensure Block 360 can be appropriately designed so that it becomes an integral component of the Boyne Survey Secondary Plan.

The development proposal for the Major Node anticipates mid-rise built form with building heights up to 7-storeys. Therefore, the UDB & ACG has been prepared with regard for the Town of Milton's approved Mid-Rise Guidelines (May 2018).



The development of Major Node Block 360 will provide a variety of intensified built forms including townhomes and apartments

1.2 VISION STATEMENT

The design vision for the subject lands is to create a mixed-use residential and commercial node that will offer a variety of higher density, transit-supportive housing options, including townhomes and mid-rise apartments with ground level commercial/retail opportunities. The proposed development provides for mixed-use intensification of this key intersection within the community and is intended to create an attractive, pedestrian-friendly, compact development in close proximity to future transit opportunities.

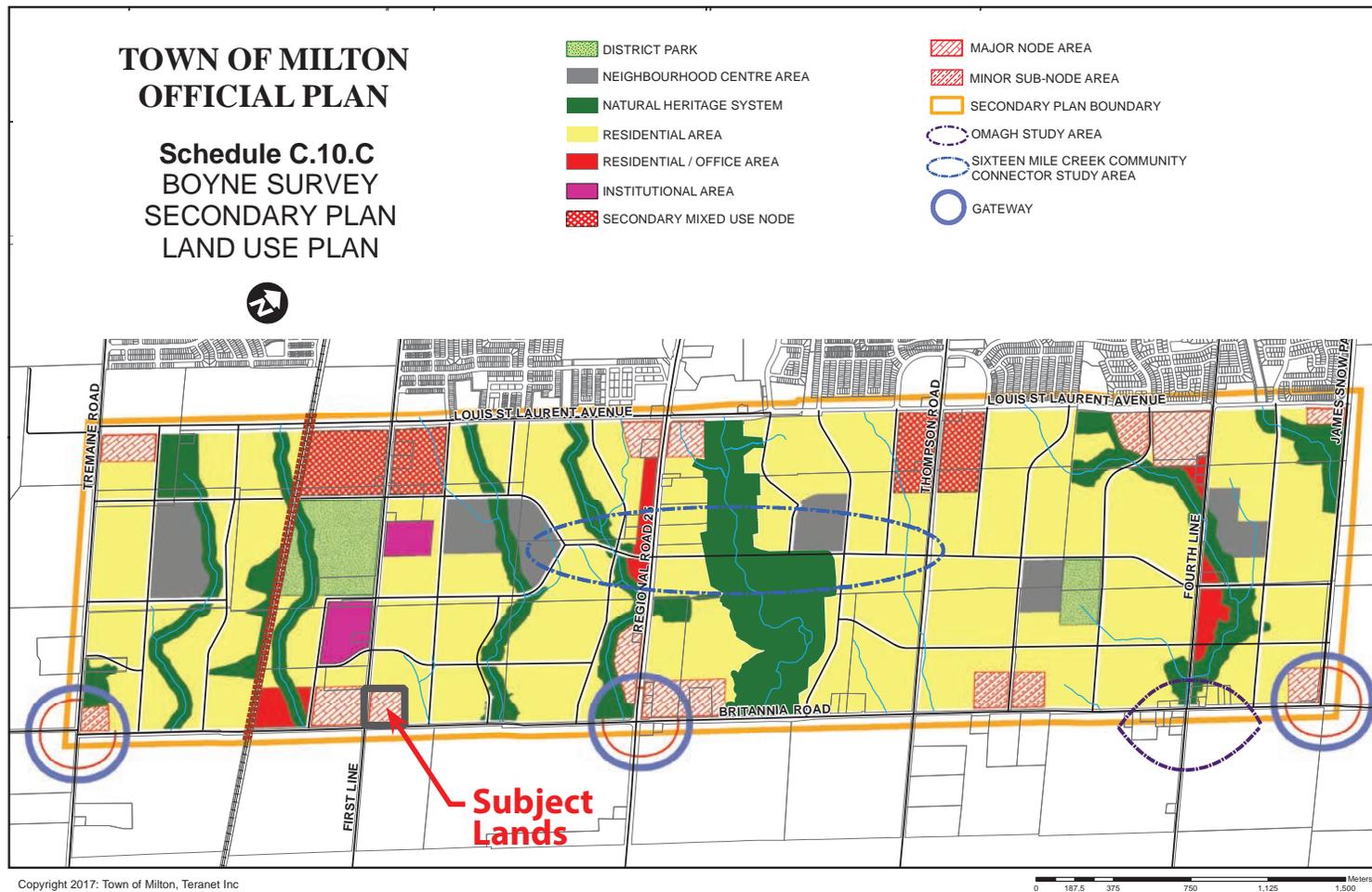
The proposed development will have a high quality, attractive built form character that will integrate with and enhance the existing neighbourhood built form context. New built form, in combination with a variety of landscape design elements, will be appropriately designed to create a sustainable and transit-supportive residential development with a vibrant visual character that will foster a distinct sense of place within the Boyne Survey Secondary Plan. The Town of Milton's "Mid-Rise Guidelines" will provide urban design guidance for the site planning and design of mid-rise buildings within Major Node Block 360.



1.3 GUIDING PRINCIPLES

1.3.1 Milton Official Plan

The subject lands are located within a “Major Node Area” as identified on Schedule C.10.C of the Town of Milton’s Official Plan. “Major Node Areas” are located at key intersections where integrated concentrations of mixed uses and higher residential densities can be accommodated. These areas will become the focus of urban activities for the surrounding neighbourhoods and should be pedestrian oriented and accessible by public transit. The permitted uses within the “Major Node Area” may include a variety of high density residential, institutional and office uses and community facilities.



Location of subject lands within Schedule C.10.C of the Town of Milton Official Plan

Outlined below are Official Plan Goals which form the basis for the various planning and urban design policies intended to ensure Milton continues to develop as a vibrant and attractive municipality.

a) To Build and Maintain a Diverse and Vital Economy;

- *This Goal recognizes the importance to Milton of providing an attractive economic environment for companies to carry on business and prosper. It also recognizes that Milton can support existing local business and attract new businesses by raising Milton's profile and identity in the wider business community in Ontario.*

b) To Build and Maintain a Safe, Livable and Healthy Community;

- *This Goal recognizes the importance to Milton of being a safe and livable community, supportive of family life and with leisure opportunities for residents of all ages.*

c) To Protect and Enhance our Heritage, Identity and Character;

- *This Goal recognizes the importance to Milton of being a well-planned community which protects its heritage and community character in both its urban and rural areas, thereby contributing to its unique sense of place and identity.*

d) To Maximize the Benefits of the Niagara Escarpment and the Natural Environment;

- *This Goal recognizes the importance to Milton of the Niagara Escarpment and its natural areas and the need to protect the air, water and land resources for future generations.*

e) To Provide Responsible Cost-Effective Local Government and Services;

- *This Goal recognizes the importance to Milton of sound local government, which is fiscally responsible and service driven.*

1.3.2 Boyne Survey Urban Design Guidelines

The proposed residential development has been designed in conformity to the objectives and design criteria outlined in the Boyne Survey Urban Design Guidelines. The Boyne Survey Urban Design Guidelines, in conjunction with this UDB & ACG, will be used by the Town in their review and evaluation of the proposed development for the subject lands through the Site Plan Approval process.

1.3.3 Main Sail Estates Architectural Control Guidelines

The "Main Sail Estates Architectural Control Guidelines (ACG)", dated December 3, 2015 were approved by the Town of Milton on December 8, 2015. As required in the ACG, this UDB & ACG has been prepared to articulate the specific design aspects of this application and will refine the design vision for this proposed development. Although the ACG deal specifically with freehold ground-related built forms, the development proposal for the subject lands and this UDB & ACG have been designed to comply with the general design objectives of the ACG and provide for maximum compatibility with neighbouring low-rise residential forms.



1.3.4 **Milton's Mid-Rise Guidelines and Milton's Shadow Impact Analysis Guidelines**

The Mid-Rise Guidelines present a number of key design principles that clarify the Town's expectations for the design of mid-rise buildings, assist with the interpretation of relevant Official Plan policies and provide a clear design direction for development proposals. The Mid-Rise Guidelines have been considered in the preparation of the development proposal for the subject lands and in the preparation of this UDB & ACG.

In addition to the above document, the design of built form and its impact on the surrounding land uses should be evaluated against the criteria within the Shadow Impact Analysis Guidelines. This document establishes minimum standards and templates to ensure that the proposed building placement, massing and height does not cause any undue shadow impacts on the public and private realm aspects of the development and surrounding context.

A shadow study for the proposed development has been prepared by RBA (Ralph Bouwmeester and Associates) and will be submitted as part of the Site Plan application. Based on the evaluation criteria set out in Milton's Shadow Impact Analysis Guidelines, the shadow study has concluded the following:

"Public Realm – Proposed shadows do not reach the opposite boulevard of Bronte Street South to the west from before 10:00AM on. Proposed shadows do not reach the opposite boulevards of Bronson Terrace to the north, Chretien Street to the east, and Britannia Road to the south. The proposal exceeds the 3-hour requirement for sunlight on the opposing sidewalks.

No sidewalk patios have been identified within the shadow reach of the proposal.

No community parks or urban plazas have been identified within the shadow reach of the proposal.

No fixed picnic stations or barbeque areas, splash pads, play equipment areas, schoolyards or community gardens have been identified within the shadow reach of the proposal.

Private Realm – Proposed shadows permit at least 2 continuous hours of sunshine (between 10:00 AM and 5:00 PM) on private front yards, rear yards, windows and rooftop patios of the surrounding residential properties.

Solar Panels – No solar panels have been identified within the shadow reach of the proposal.

In summary, this proposal meets the Shadow Impact Analysis Guidelines of the Town of Milton."

Based on the latest site plan concept that proposes a 7-storey mid-rise mixed use building, RBA has prepared a Shadow Impact Opinion Letter, dated January 23, 2026, and concludes with the following:

"The reduction of 5/6 floors results in shadow length decreases of 37% for the 7-storey portion and 48% for the 6-storey end. As a result of these reductions, the 9:00 AM shadows no longer reach the opposite boulevard of Bronte Street but rather end just beyond the middle of the street.

The conclusion (Section 10) of our Sep 17/25 shadow study remains unchanged."



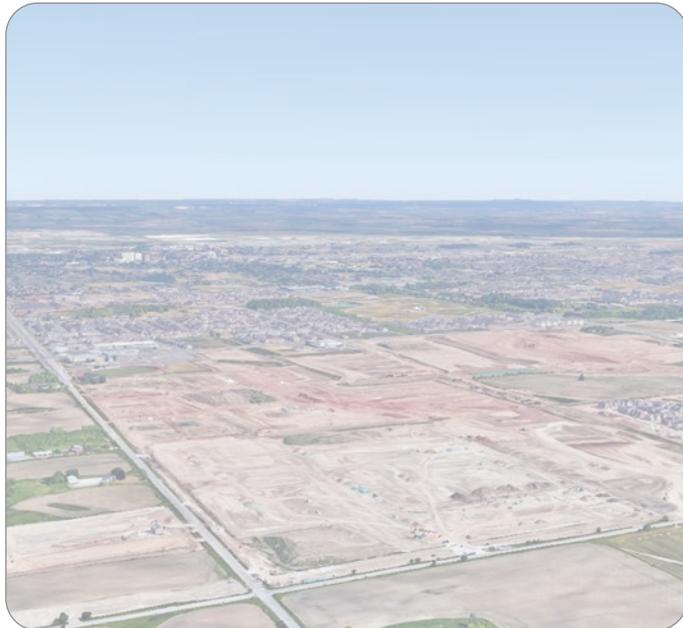
1.4 OBJECTIVES

The following objectives will be used to support the vision statement and guide urban design decisions related to the proposed development of the subject lands:

- Provide a transit-supportive urban form that helps sustain local and regional transit initiatives in accordance with the **Town Urban Design Guideline Section 3.3.1 Transit-Supportive Design**.
 - Design a compact development that appropriately integrates with the built form, streetscape and road network of the surrounding uses.
 - Promote innovative mixed uses and higher residential densities that contribute to the diversity of housing options within the community.
- To encourage an attractive, pedestrian-scaled streetscape that promotes a sense of place along Bronte Street South, Britannia Road, Bronson Terrace and Chretien Street through the design of new housing, dwelling facade variety, building materials, colour palette, and landscaping.
 - To establish requirements for the appropriate siting of buildings according to type, size, style and location within the development.
 - To encourage public safety by promoting the principles of CPTED (Crime Prevention Through Environmental Design).
 - To minimize the visual impact of garages and parking areas within the streetscape.
 - Encourage sustainable development practices in the siting and design of new buildings.



Attractive built form, landscaping and streetscape elements will contribute to a pedestrian-scaled development

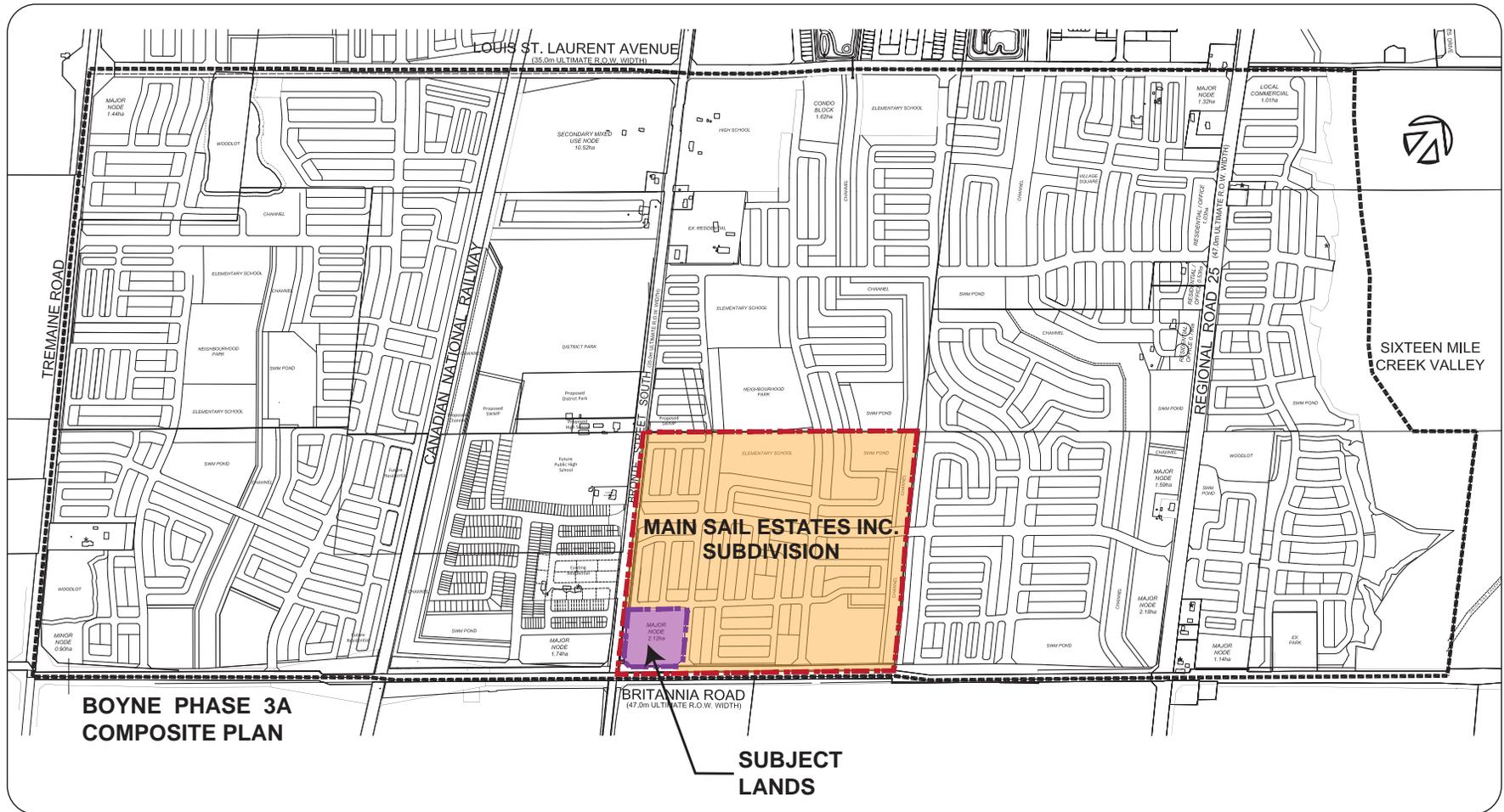


PART 2

SITE AND CONTEXT ANALYSIS



Outlined in the following sections is an analysis of the site, including opportunities and constraints that will be considered in the urban design approach for the proposed development. Shown below is a plan highlighting the location of the subject lands within the Boyne Phase 3A Composite Plan.



Location of the Subject Lands (Major Node Block 360) within the Boyne Phase 3A Composite Plan

2.1 SITE FEATURES

The subject lands are located in the northeast corner of Bronte Street South and Britannia Road and occupy an area of approximately 2.11 hectares (5.2 acres).

The subject lands are bounded by the following:

- North: Bronson Terrace; further north is Carr Landing and existing street townhouses.
- East: Chretien Street; further east are existing street townhouses.
- South: Britannia Road; further south are existing agricultural lands.
- West: Bronte Street South; further west are existing agricultural lands to be developed for a future major node block.
- Refer to site context images on the following pages.

Once this area is fully developed, the site will be in close proximity to local amenities such as public transit, bike routes, parkland, commercial / service and recreational uses which will contribute to the transit-supportive nature of this development by providing residents with the opportunity to be less dependant upon automobile usage.

The site is generally level and is currently vacant and most of the stockpile of top soil has been removed. The site has previously been cleared as part of the development of the surrounding lands and contains no significant vegetation.



Major Node Block 360 - Key Plan

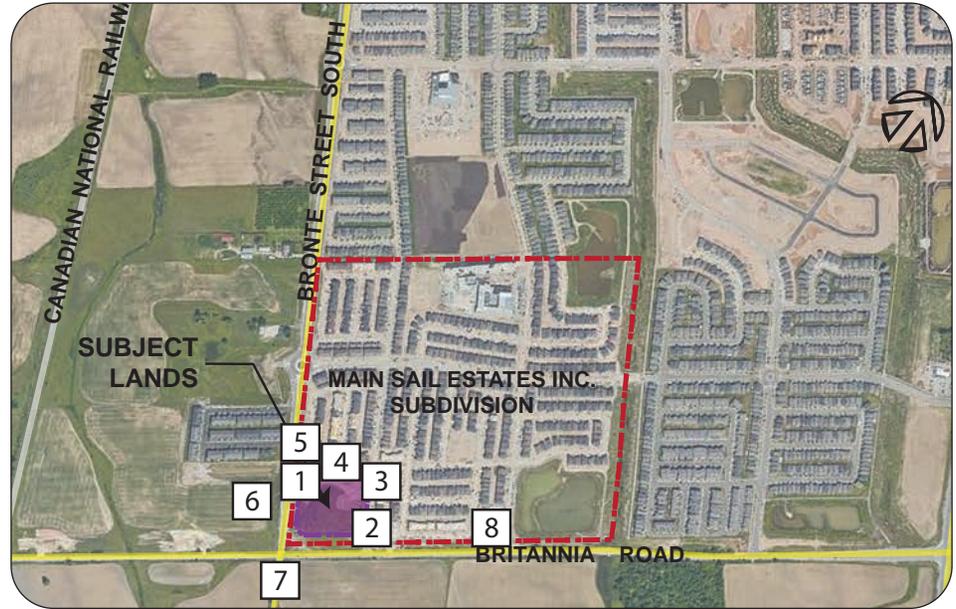
Source: Google



View of the subject lands from Bronte Street S. & Bronson Terrace



View of the site from Britannia Road and Chretien Street



Existing Site Context / Built Form Images

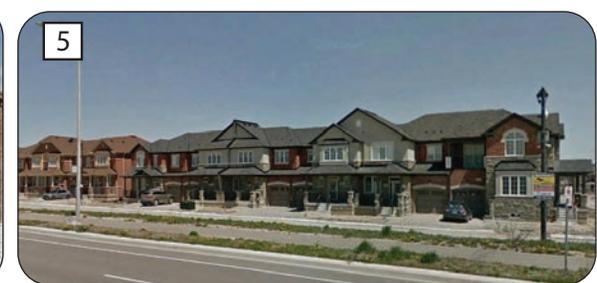
Source: Google



View of existing townhomes along Chretien Street



View of existing townhomes along Bronson Terrace



View of existing built form along Bronte Street South



View of lands on the west side of Bronte St. S.



View of lands on the south side of Britannia Road



View of existing commercial plaza at Britannia Rd. & Leger Way



2.2 LOT FABRIC / BLOCK PAT- TERN / SPATIAL ANALYSIS

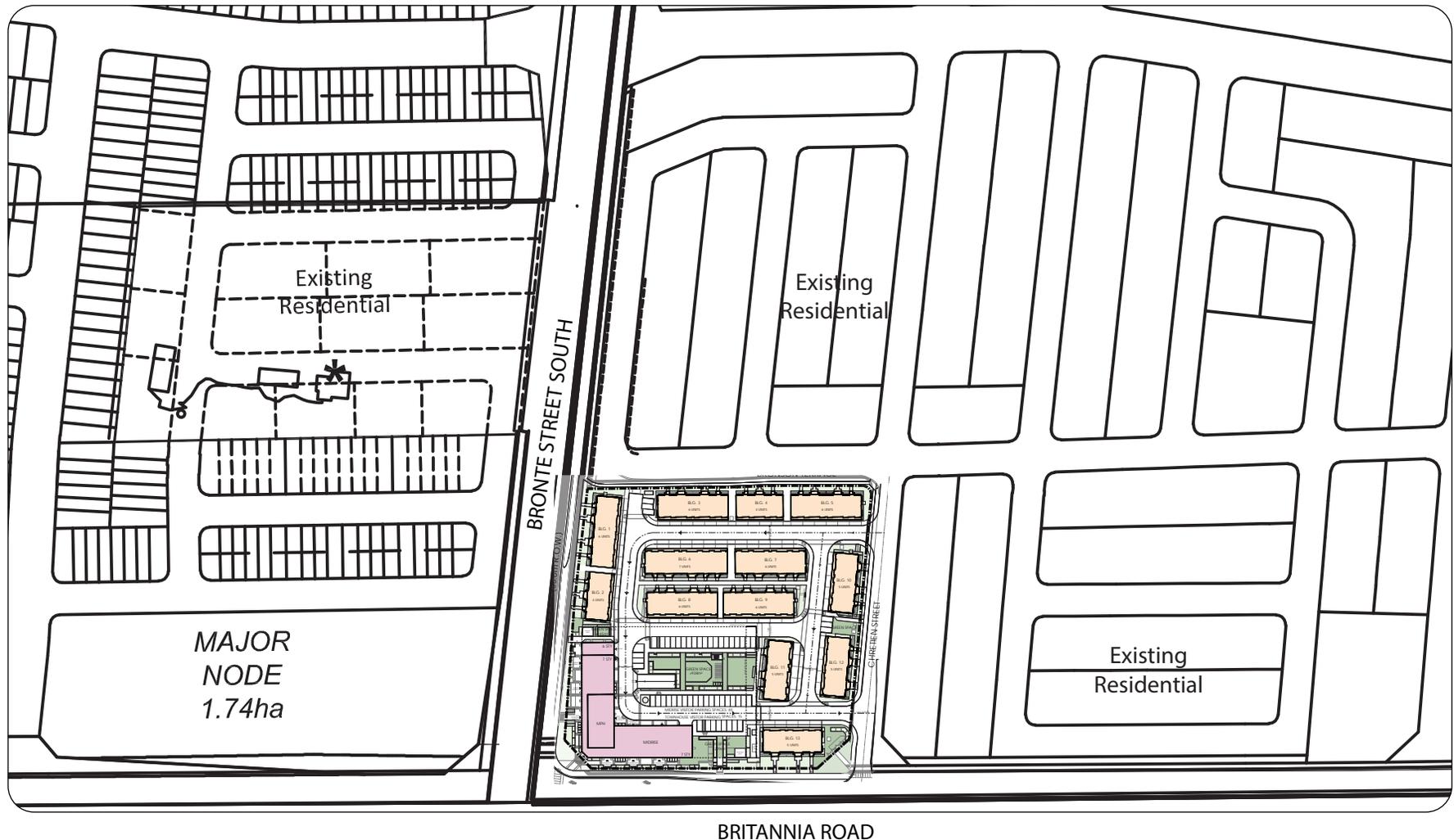
- The site is approximately 144.7m wide X 147.8m deep, this includes:
 - 116.41m of frontage along Britannia Road, plus daylight triangles;
 - 129.94m of frontage along Bronte Street South, plus daylight triangle;
 - The site also has frontage on Bronson Terrace and Chretien Street.
- A street pattern consisting of 7.0m wide private roads will allow vehicular, bicycle and pedestrian movement throughout the site and will take access from Chretien Street from two different points.
- The proposed built form located along the perimeters of the site establishes the block pattern and road network throughout the development.
- A mid-rise apartment building occupies the intersection of Bronte Street South and Britannia Road. This building will contain approximately 808m² (8697.24 sq.ft.) of commercial/retail space on the ground level.
- To maintain a pedestrian scale, the mid-rise building has incorporated appropriate setbacks and building setbacks together with generous open space areas between the adjacent townhouse blocks which allow for views and connectivity into the development.
- Double Car Garage Townhouses are proposed north and east of the mid-rise



Concept Plan Illustrating General Street / Block Pattern

building, fronting onto Bronte Street South and Britannia Road, and internal to the development block.

- Townhouse block lengths vary from approximately 24.8m to 36.8m.
- Lot depths range from 16.6m (excluding balcony projections) for the apartment building to 22.1m for double car garage townhouses.
- The diagram on this page shows how the proposed development will fit into the planned context of the community, illustrating its spatial relationship to neighbouring uses.



Concept Plan in Context to Surrounding Planned Development

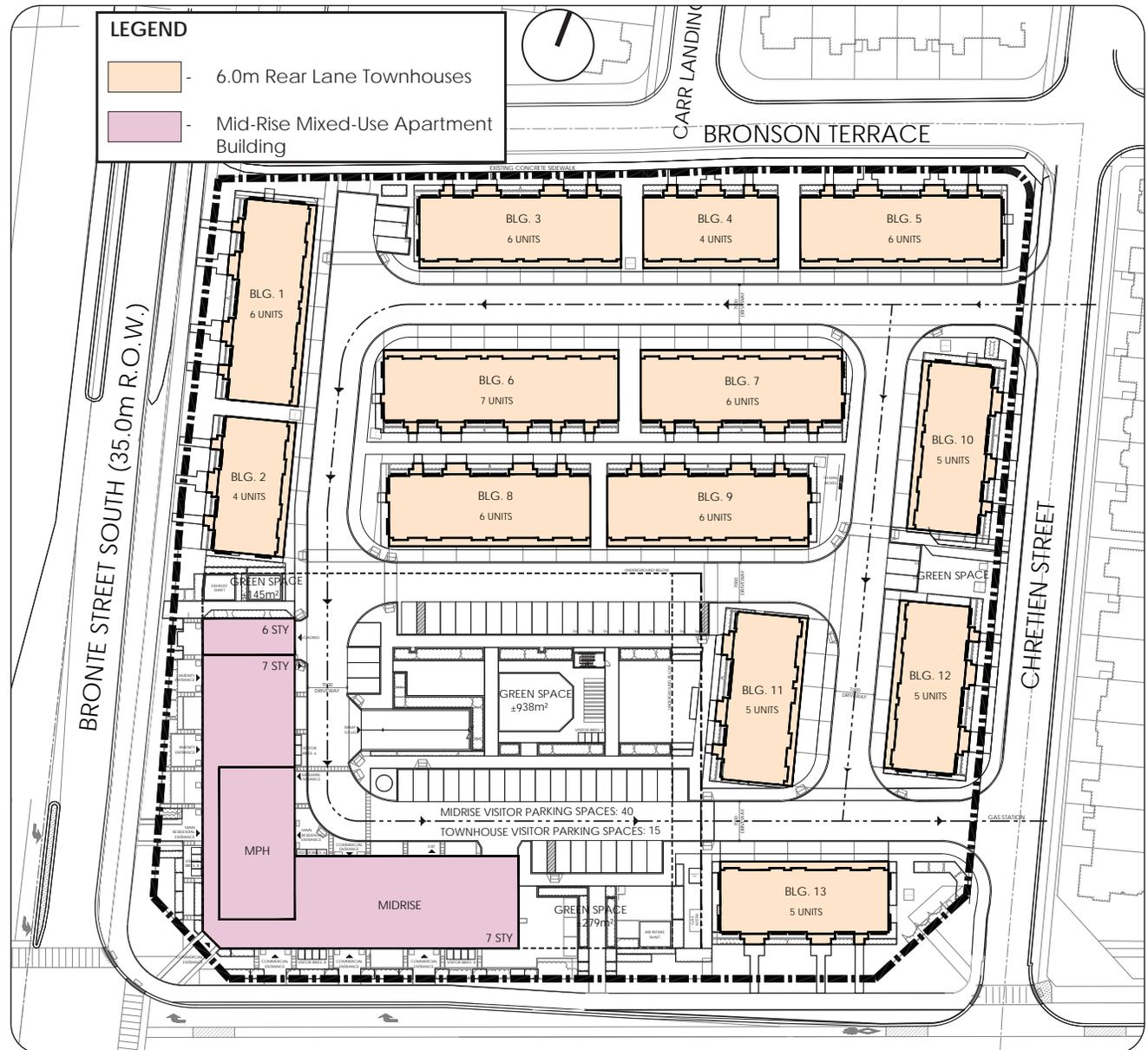
2.3 BUILT FORM ANALYSIS

There will be a total of 223 units (152 high density units and 71 TH units). Three building types are proposed:

1. Thirteen blocks (71 units) of 3-storey, 6.0m wide Double Car Garage Townhomes (Buildings 1 - 13) with attached rear-accessed double-car garages, allowing for 2 parking spaces per unit; private outdoor space for these dwellings will be on elevated patios above the garage and rooftop terraces.
2. A 7-storey mid-rise apartment building (with 152 units) with an underground parking garage and ground level commercial/ retail space. Apartment units will have outdoor private space in the form of balconies, terraces or patios.

The apartment building will be a conventional condominium tenure. The the double car garage townhouses will be a POTL (parcel of tied land) tenure with condo road (i.e. Common Element Condominium).

Refer to Section 3 (Design Response) for further detail on each building type.



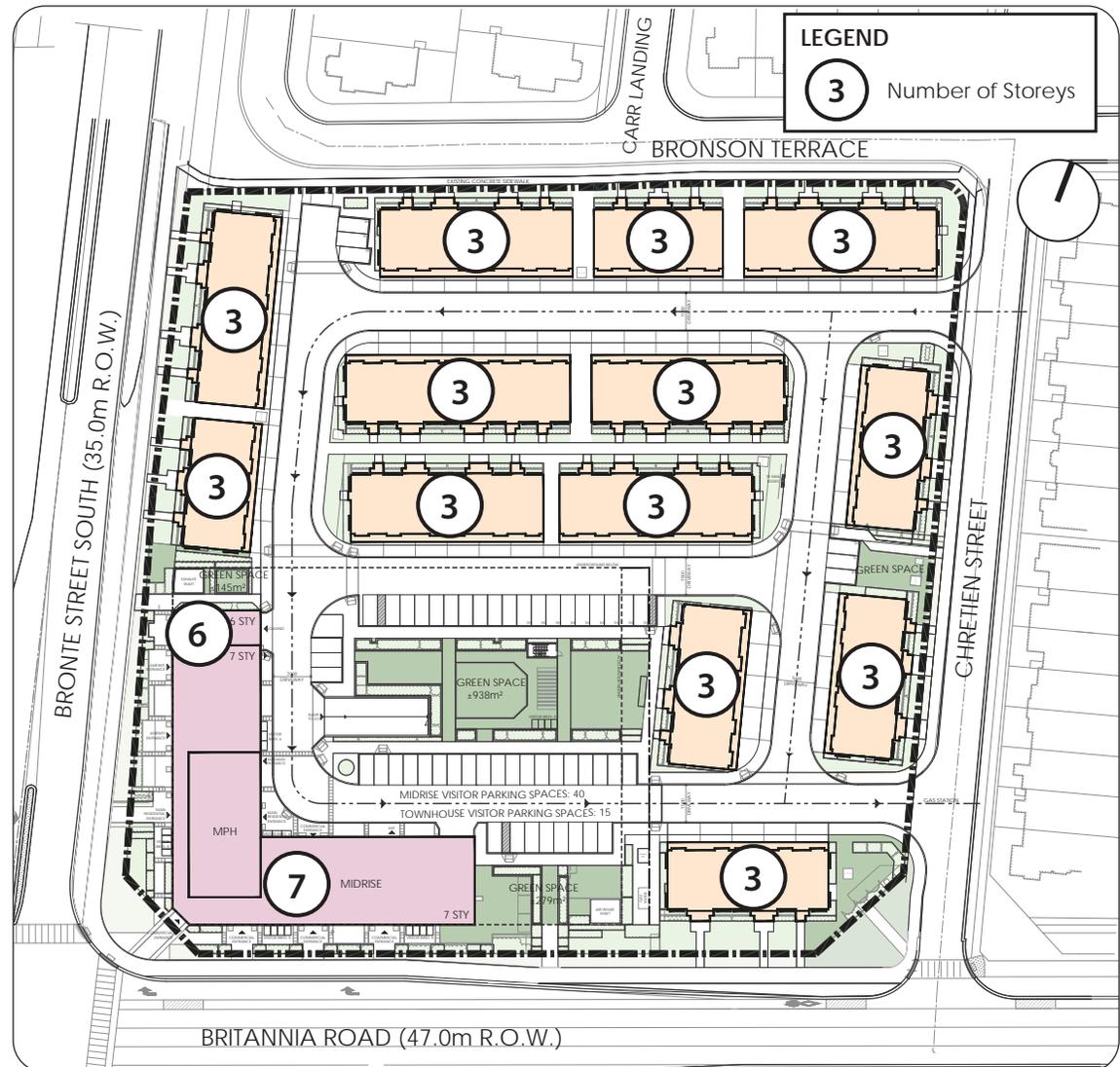
Built Form Plan

2.4 RELATIONSHIP WITH SURROUNDING LAND USES

- The surrounding area is currently transitioning from rural to urban. Land uses consist primarily of residential uses and limited commercial use. Existing townhouses and a future major node block are located in the immediate area.
- 2-storey townhouses are located to the north, east and west of the site. A future Major Node block is located at the northwest corner of Bronte Street South and Britannia Road, opposite the subject lands.
- The lands south of Britannia Road lie outside of the Urban Boundary and will remain as agricultural lands in the short term. Future urban development is possible to the south at such time as the Urban Boundary is expanded.
- The apartment building (7 storeys + mechanical penthouses) is situated at the intersection of Bronte Street South and Britannia Road to create a dominant built form presence. Moving to the north and east away from the intersection, 3-storey townhouses are proposed for massing compatibility with neighbouring townhouses on the opposite side of Bronson Terrace and Chretien Street.

2.5 VIEWS AND VISTAS TO AND FROM SITE

- The site is situated in a prominently visible location within the community. Views of the development will occur on all four sides from Bronte Street South, Britannia Road, Bronson Terrace and Chretien Street.
- Given this high degree of exposure, the design of building elevations in areas of high public visibility shall have enhanced architectural features to create attractive views along these edges.



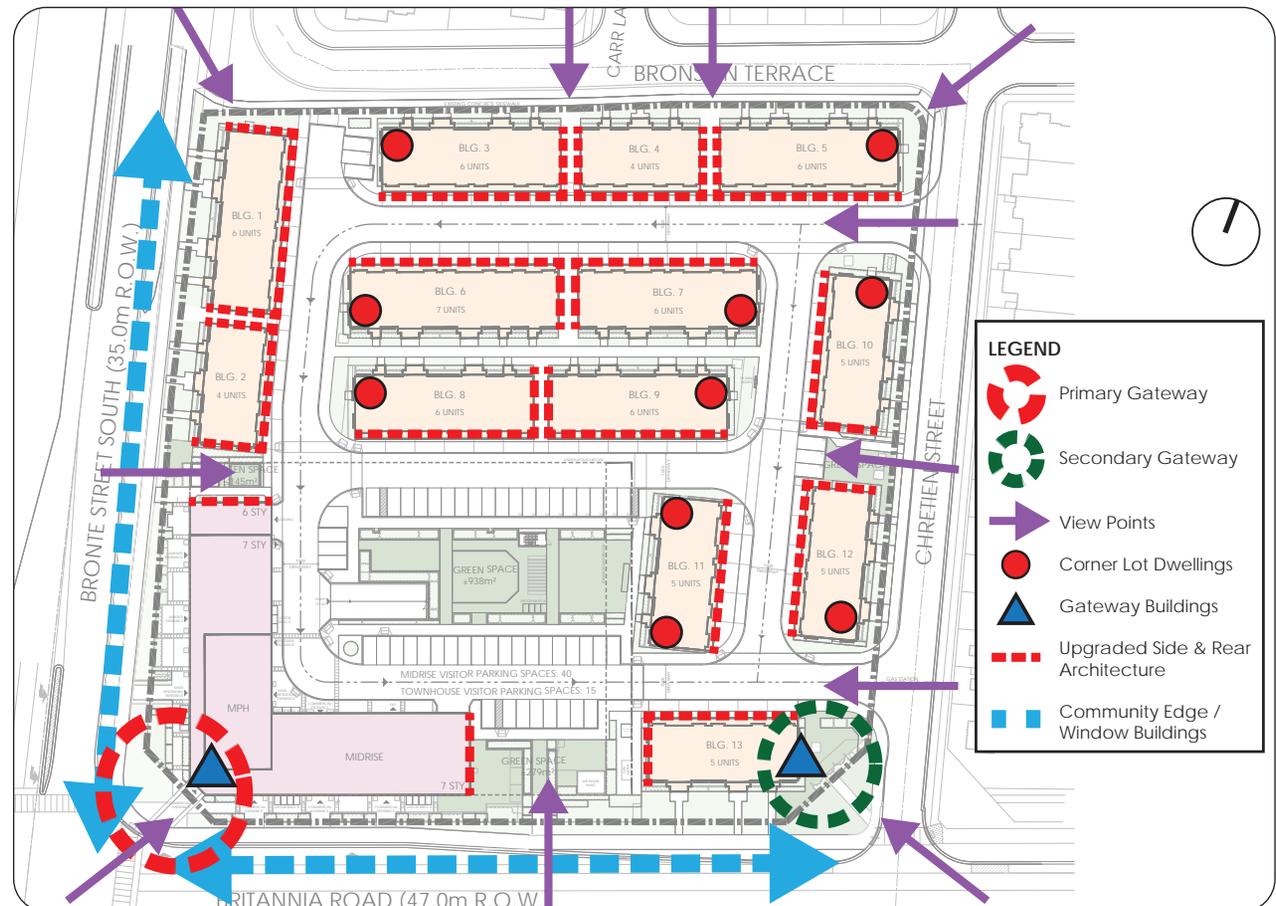
Building Heights in Relation to Neighbouring Built Form

2.6 GATEWAYS, VIEWS AND FOCAL LOCATIONS

- The proposed development shall exhibit design excellence in built form and landscaping to create an appropriate landmark character and place-making identity at the intersection of Bronte Street South and Britannia Road.
- The tallest portions of the proposed buildings should be oriented to the intersection of Bronte Street South and Britannia Road, gradually stepping down in height for compatibility with neighbouring townhouse forms.
- The use of walled 'gateway' features is discouraged. Instead the built form and landscaping shall be used to provide design emphasis at gateway locations.
- Implement gateway features such as civic plazas or landscaped areas at the intersection of Britannia Road and Bronte Street South.
- Other buildings at 'focal locations' within the proposed development include:
 - corner lot townhouses which require special design consideration to ensure flankage elevations have additional fenestration, wall / roof form articulation and upgraded architectural detailing similar to the front elevation.
 - buildings abutting open space areas, walkways and the ramp to the underground parking garage require additional fenestration, wall / roof form

articulation and upgraded architectural detailing similar to the front elevation.

- the rear elevations of double car garage townhouses facing the private lane will be well-articulated through the use of wall projections, windows, balconies, terraces and materials, and will be designed to ensure surveillance and overlook of the lane through ample glazing and balconies.



Gateways, Views and Focal Locations

2.7 INTERFACE BETWEEN PUBLIC REALM AND PRIVATE REALM

- A centralized outdoor green space (+/-938m²) will be provided to serve as the primary recreational feature and social gathering space for all residents within the proposed development. Two smaller green spaces (+/-279m² & +/-145m²) are located between the mid-rise building and proposed townhouse buildings (Buildings 2 & 13). There are also three minor green spaces located at the main entrance to the development from Britannia Road, between Buildings 10 and 12, and between Buildings 6/7 and 8/9. Combined, a total of 1362m² of common outdoor open space is provided for the development. Refer to Sec. 3.2.4 for further details.
- The central outdoor green space will have private road frontage along three edges, and on the eastern edge, it will face the front facades of the proposed double car garage townhouse block (Building 11). Buildings visible from the green space should be well-articulated with large amounts of glazing to create an attractive backdrop to the outdoor open space and foster overlook of the public realm. Likewise, the facade treatments for mid-rise buildings abutting open space parcels shall also be enhanced to ensure attractive views.
- Landscaping and fence treatments should be designed to maximize natural surveillance of the outdoor open spaces from the adjacent dwellings



Built form fronting onto the outdoor open space shall include ample fenestration, articulation and architectural detailing to serve as an attractive backdrop to this space

while providing a defined separation between public and private space and allowing for privacy for the adjacent homeowners.

- The mid-rise building will interface directly with Britannia Road and Bronte Street South. This building shall be designed with active front facades and entrances facing these major roadways to foster vibrant pedestrian-oriented streetscapes.

2.8 TRANSPORTATION NETWORKS

2.8.1 Vehicular and Pedestrian Access and Circulation

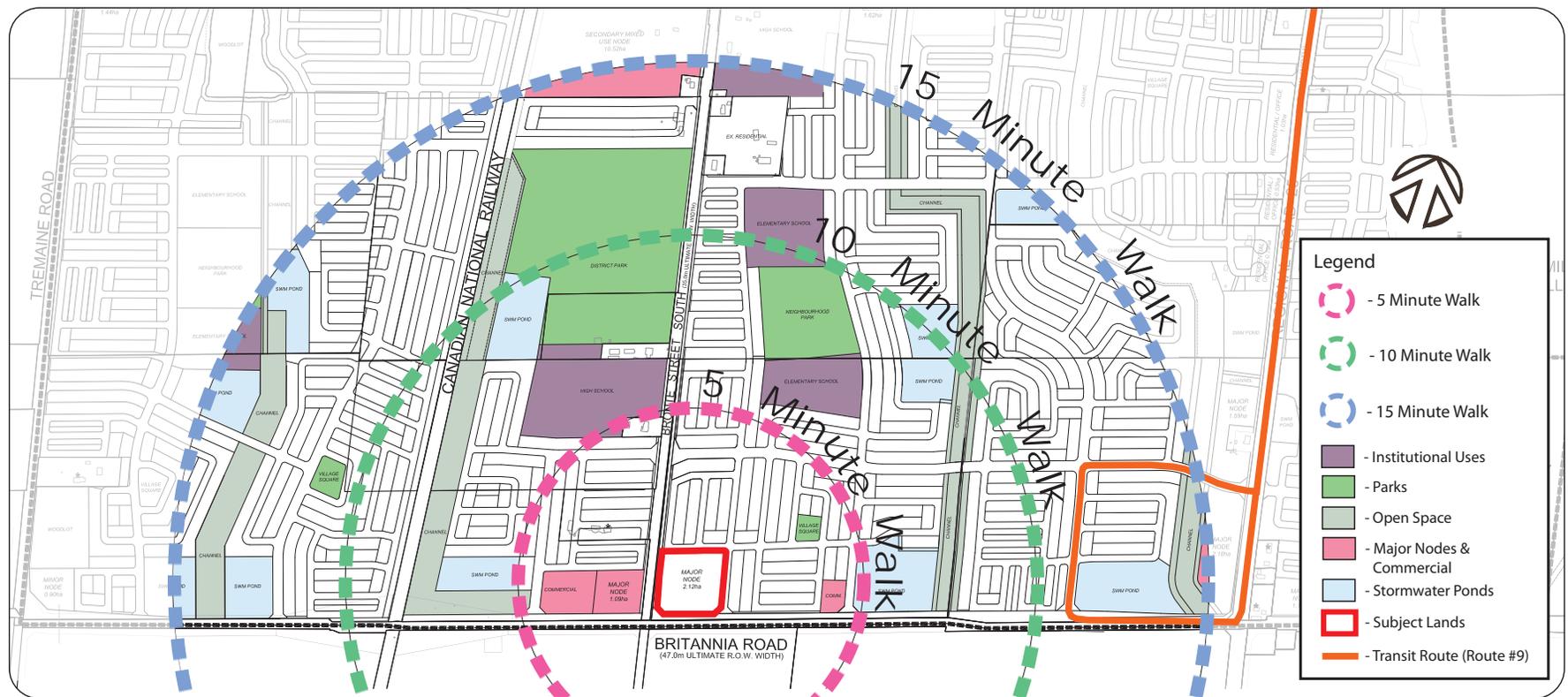
- The site is at the intersection of 2 major arterial roads:
 - Britannia Road (with an ultimate R.O.W. of 47.0m);
 - Bronte Street South (with an ultimate R.O.W. of 35.0m).
- Due to the site's proximity to this major intersection, vehicular access points to the site will occur from Chretien Street via two 7.0m wide private roads.
- These private roads provide access to all double car garage townhouse units and the underground parking area for the apartment building.
- The site design maximizes permeability and connectivity for vehicular, bicycle and pedestrian movements.
- Walkways will be provided leading to all building entrances, common outdoor open spaces, parking areas and to adjacent streets and sidewalks.
- Refer to Section 3.2.2 for Vehicular and Pedestrian Circulation Plan.

2.8.2 Transit Network

- This area of Milton is served by Milton Transit’s “9A/B - Ontario South” bus routes. Once this area is urbanized, greater transit opportunities will follow as outlined in the 2019-2023 Milton Transit Services Review & Master Plan Update (June 2019).
- A potential bus pad location has been identified on Britannia Road near the intersection with Bronte Street South. Its final location shall be confirmed with Milton Transit.

2.8.3 Walkable Neighbourhood Facilities

- The subject lands are within a 5-10 minute walking distance of existing/ proposed neighbourhood facilities including parks, schools, open space areas, commercial and major node uses, as shown on the plan on the Walkable Neighbourhood Plan.
- The street and block pattern, both within the proposed development and throughout the adjacent existing subdivisions, has been designed to provide tree-lined, walkable blocks that respond to pedestrian desire lines. This is intended to result in a pedestrian-friendly neighbourhood that is well-connected and fully accessible to the community’s major destinations and to surrounding neighbourhoods.
- Street block lengths do not exceed 250 metres, which is considered an



Walkable Neighbourhood Plan

urban design metric that supports ease of walkability and permeability through the neighbourhood.

- Open space parcels are linked through the street, sidewalk and walkway network to form a continuous, complete and pedestrian-friendly public realm.
- Streetscape elements, pedestrian-oriented spaces, landscaping and interesting architecture will be used to create a safe and comfortable environment that promotes active transportation.
- Bicycle parking shall be provided in convenient locations to promote active transportation, including storage spaces in the underground parking garage and aboveground bicycle racks located close to the main entries of the mid-rise buildings.
- Refer to Active Transportation Plan in Section 3.2 for conceptual pedestrian routes and linkages through the site.

2.9 OPPORTUNITIES AND CHALLENGES

2.9.1 Site Opportunities

- Opportunity to intensify an underutilized site at a major intersection with the Town of Milton.
- Opportunity to create a transit-supportive mixed-use development through appropriate building and site design.
- Opportunity to provide ground level commercial space for the mid-rise apartment building.
- Opportunity to create a community gateway at the intersection of Britannia Road and Bronte Street South through design of built form and landscaping.
- Opportunity to create pedestrian friendly streetscapes along Britannia Road and Bronte Street South. This will occur through built form that appropriately addresses the street and landscaped edges.
- Opportunity to create a well-connected community that supports active transportation. This will occur through pedestrian/cyclist permeability through the site, defined walkways, connectivity to adjacent streets and sidewalks / trails and provision of bicycle parking.

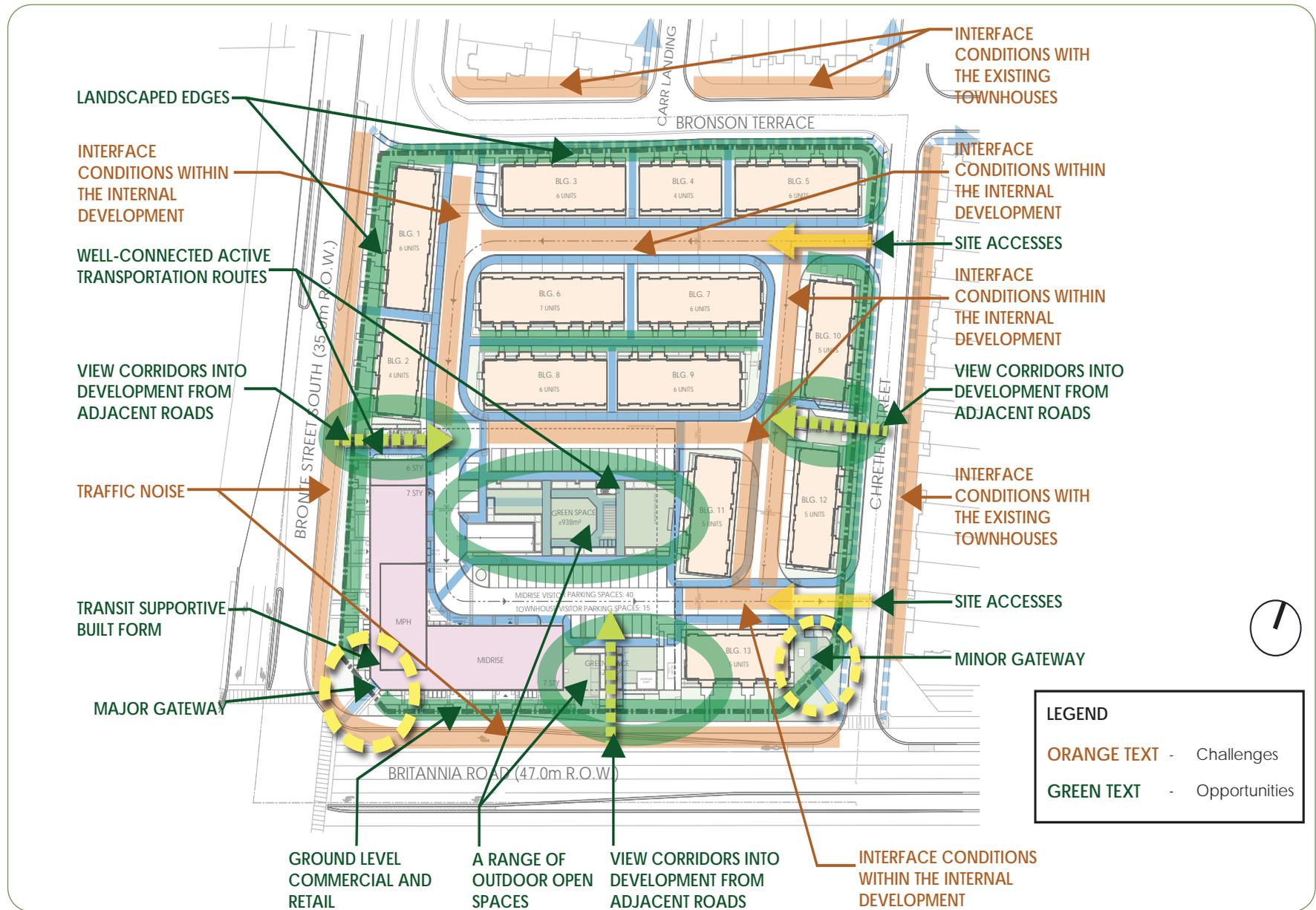
- Opportunity to create view corridors into the site from the perimeter roads.
- Opportunity to provide a range of outdoor open spaces for the benefit of residents / patrons of the proposed development.
- Opportunity to locate main parking areas underground.

2.9.2 Site Challenges

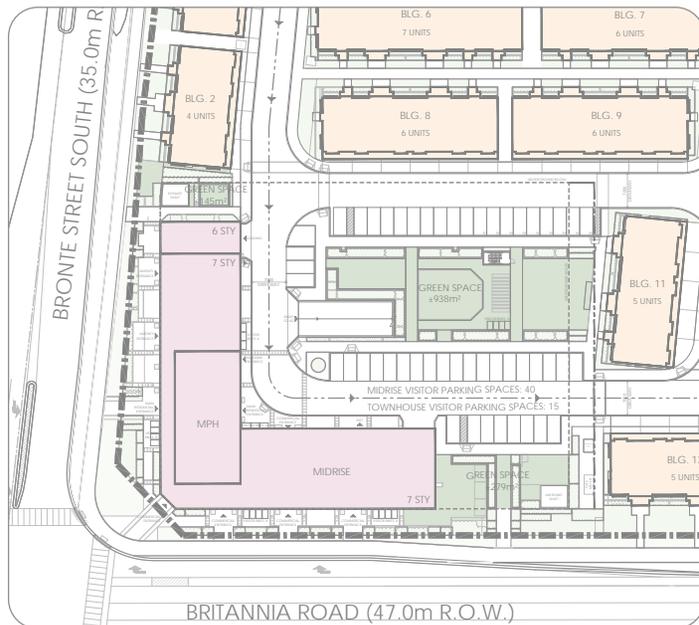
- Site access from Britannia Road and/or Bronte Street South is limited.
- Limited transit options in the local area.
- Noise and traffic levels along Britannia Road and Bronte Street South may require special design considerations made in accordance with the Noise Report prepared by Jade Acoustics.
- Proximity to existing residential uses adjacent to the site will require an appropriate built form interface that will impact building heights.

Refer to the Opportunities and Challenges Plan on the following page.





Opportunities and Challenges Plan



PART 3

DESIGN RESPONSE AND CONCEPT PLAN

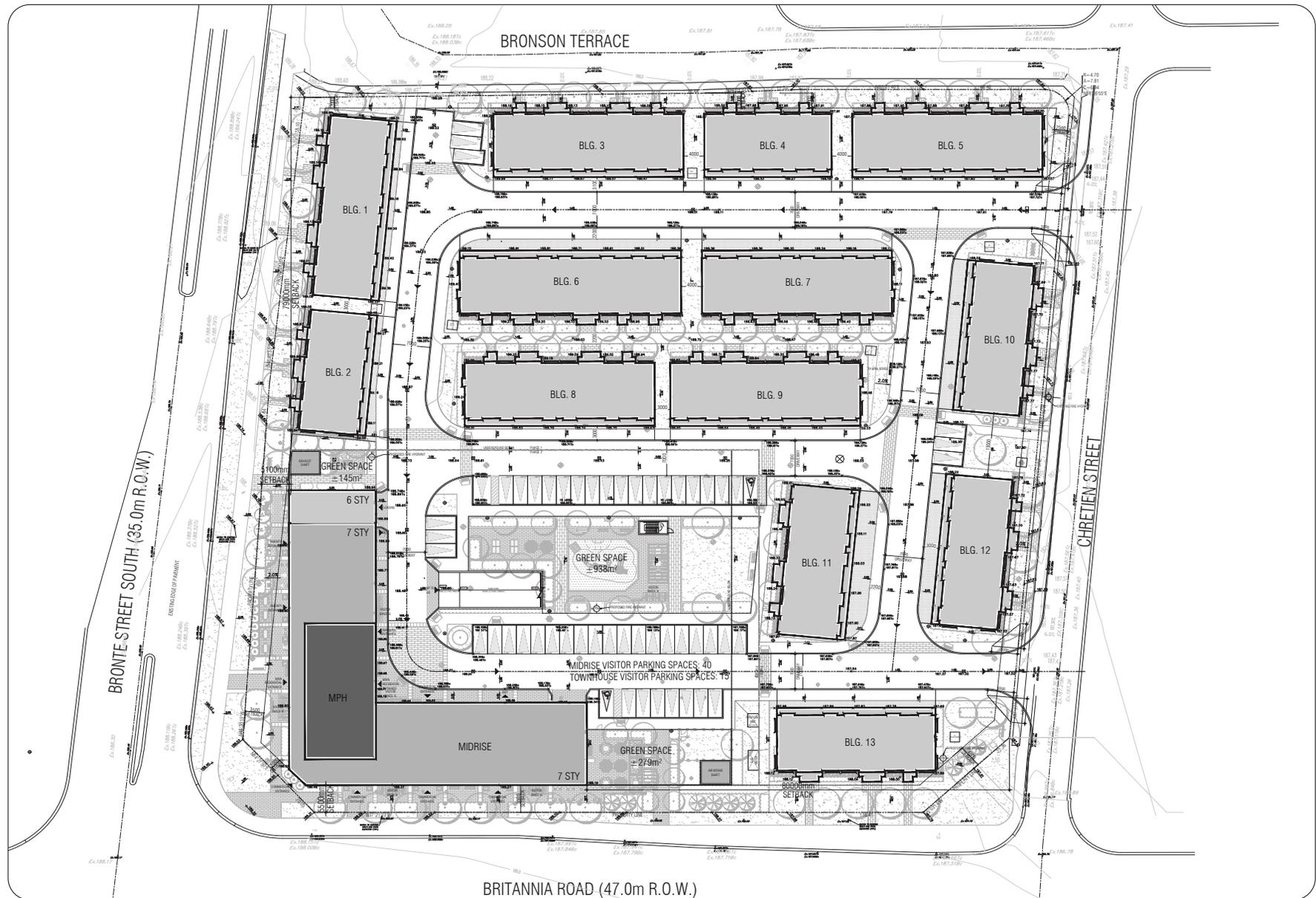
3.1 SITE LAYOUT AND DESIGN

The UDB & ACG provides a conceptual design approach for development of the private and public realm within this proposed Major Node development. It provides sufficient detail to provide the Town with assurances that the development proposal for Major Node Block 360 will appropriately integrate with the existing and planned surrounding context and provide for distinct place-making built form that will create a landmark identity in this key location of the community.

The proposed condominium development concept will provide for the following:

- A pedestrian-scaled environment will be achieved by incorporating height and massing that is appropriate to the context of the adjacent streets and the surrounding area. Prominent building massing will be oriented toward the Britannia Road and Bronte Street South intersection to create a strong 'gateway' to the community while maintaining an appropriate relation to adjacent grade-related housing forms.
 - A series of open space blocks will provide passive recreation areas for residents, allow for views to and from the development and provide a high degree of pedestrian connectivity into and throughout the development.
 - The primary building face for the mid-rise building will be sited parallel and close to the adjacent arterial roads to reinforce an active pedestrian zone and strong street edges. In order to maintain a comfortable pedestrian scale, the maximum length of the a main wall shall not exceed 60m.
 - High quality, articulated building façades will create animated streetscapes, a vibrant public realm and pedestrian-scaled environments throughout the proposed development.
 - Building entrances facing the adjacent public streets will be provided for the mid-rise and townhouse buildings. Entrances will be connected to the public sidewalk by a hard surface walkway. Building entrances will also be provided facing the private road / passenger drop-off areas.
 - Although the building forms may vary, a similar vocabulary of materials and architectural elements should be employed throughout the development area to create visual stimulation while maintaining continuity.
- Projections into the front yard / street zone, such as entrance canopies, porticos, window bays and pilasters, are encouraged for their beneficial impact on creating an animated streetscape.
 - For buildings on corner lot locations, both street frontages shall be addressed in a similar and appropriate manner.
 - Positive relationships between built form and public spaces shall be maintained throughout the design process to yield high quality streetscapes and architectural excellence.
 - A comprehensive streetscape image will be achieved through the combination of appropriate built form, massing, architectural details and landscaping. Positive relationships between built form and public spaces shall be maintained throughout the design process to yield high quality streetscapes and architectural excellence.
 - The road network for the proposed development is defined by the existing road fabric and surrounding land uses. Streets shall provide for the safe and convenient movement of pedestrians, vehicles and cyclists, fulfill their role as a place where social interaction may occur, and contribute to the visual impression of the community. A network of sidewalks will be integrated into the site plan configuration to reinforce the vision of a pedestrian-oriented neighbourhood with multiple linkage opportunities.
 - Main parking areas for the apartment building is located within an underground parking structure. Limited surface parking areas are distributed through the site for visitors.
 - Double car garage townhomes will have 2-car garages fully integrated into the massing of the dwelling.
 - Loading, service and garbage areas will be located in unobtrusive area away from public view.
 - Transformer and/or telecommunications vaults and pads will be located to avoid negative impact on the streetscape.





Major Node Block 360 - Concept Plan (Prepared by Graziani + Corazza Architects)

Job #:2268.24 - Shearling Heights (Britannia + Bronte, Milton)											
Project Statistics				As Required by By-Law 074-2022 and By-Law 009-2025				Proposed Site Plan Statistics			
Site Area											
Overall							21,103	m2			
							2.11	ha.			
							5.2	ac.			
F.S.I.											
Overall Site			1.79						1.26		
Landscape											
Landscape Open Space									40.5%		
Unit Count											
Mid-Rise									152		
Multiple Dwellings									71		
Total									223		
Mid-Rise Parking											
Residential (underground)			Ratio	Units	Total		Ratio	Units	Total		
			1	152	152		1	152	152		
Commercial/Visitor (at grade)			0.2	152	31		0.2	152	31		
Total					183				183		
Multiple Dwellings											
DBL Car Garage	Res.		2	71	142		2	71	142		
Visitor Parking (Provided in lay-by's at grade)	Vis.		0.2	71	15		0.2	71	15		
Total					157				157		
Extra Parking											
Visitor (at grade)									9		
Total									9		
Overall Total			Total		340		Total		349		
Barrier Free Spaces			Required:		9		Provided:		9		
% of grade level surface parking									4%		
Proposed 7 Sty Bldg. (Mid-Rise)			Zoning Approved Plans as of July 6th, 2022					Provided			
G.F.A.											
Commercial					903 m ²				808 m ²		
Residential					17621 m ²				10296 m ²		
Total					18,524 m²				11,104 m²		
Setbacks *											
Above Grade											
Rear Yard (Bronson Terrace)					3.3m				3.3m		
Exterior Side Yard (Chretien Street)					17.5m				17.5m		
Front Yard (Britannia Road)					5.5m				5.5m		
Front Yard (Bronte Street)					3.5m				3.5m		
Below Grade											
North					2.5m				2.5m		
East					2.5m				2.5m		
South					5.5m				5.5m		
West					3.5m				3.5m		
Unit Count											
Total					MAX. 364 UNITS				152		
Bike Parking											
Residential			Ratio	Units	Comm.Parking	Total	Ratio	Units	Comm.Parking	Total	
			0.2	152		73	1.0	152		152	
Visitor / Commercial (At Grade)			0.03		31	1	0.29	152	36	44	
Total						74				196	
Building Height **					MAX. 12 STOREYS / 42.0m			7 STRYS		24.83 m + 5.65m MPH	
Amenity Space											
Outdoor Open Space					3959m ²				1362 m ²		
Indoor Amenity					671m ²				204 m ²		
Proposed 3 Sty Multiple Dwellings			Zoning Approved Plans as of July 6th, 2022					Provided			
G.F.A.											
Building 1									1,281 m ²		
Building 2									1,067 m ²		
Building 3									1,281 m ²		
Building 4									1,067 m ²		
Building 5									1,281 m ²		
Building 6									1,493 m ²		
Building 7									1,281 m ²		
Building 8									1,281 m ²		
Building 9									1,281 m ²		
Building 10									1,070 m ²		
Building 11									1,070 m ²		
Building 12									1,070 m ²		
Building 13									1,070 m ²		
Total									15,590 m²		
Unit Count											
Total					MAX. 60 Multiple Dwelling units				71		
Building Height **					13.5m			3 STRY		11.805 m	
*Setbacks to main building face											

Major Node Block 360 - Site Statistics (Prepared by Graziani + Corazza Architects)





Major Node Block 360 - Conceptual Perspective Showing Building Massing



3.2 PUBLIC REALM FRAMEWORK

3.2.1 Streetscape Elements

Within the proposed condominium development, the various elements within the streetscape help to define its character and image. The consistency and placement of these elements must work well in consideration of safety and functional objectives to complement the proposed built form and provide comfort for pedestrian activities.

Streetscape elements within the Major Node development will consist of the following:

- Roads / Sidewalks
- Street Trees / Landscaping
- Community Mailboxes
- Screening /Fencing
- Lighting
- Utilities

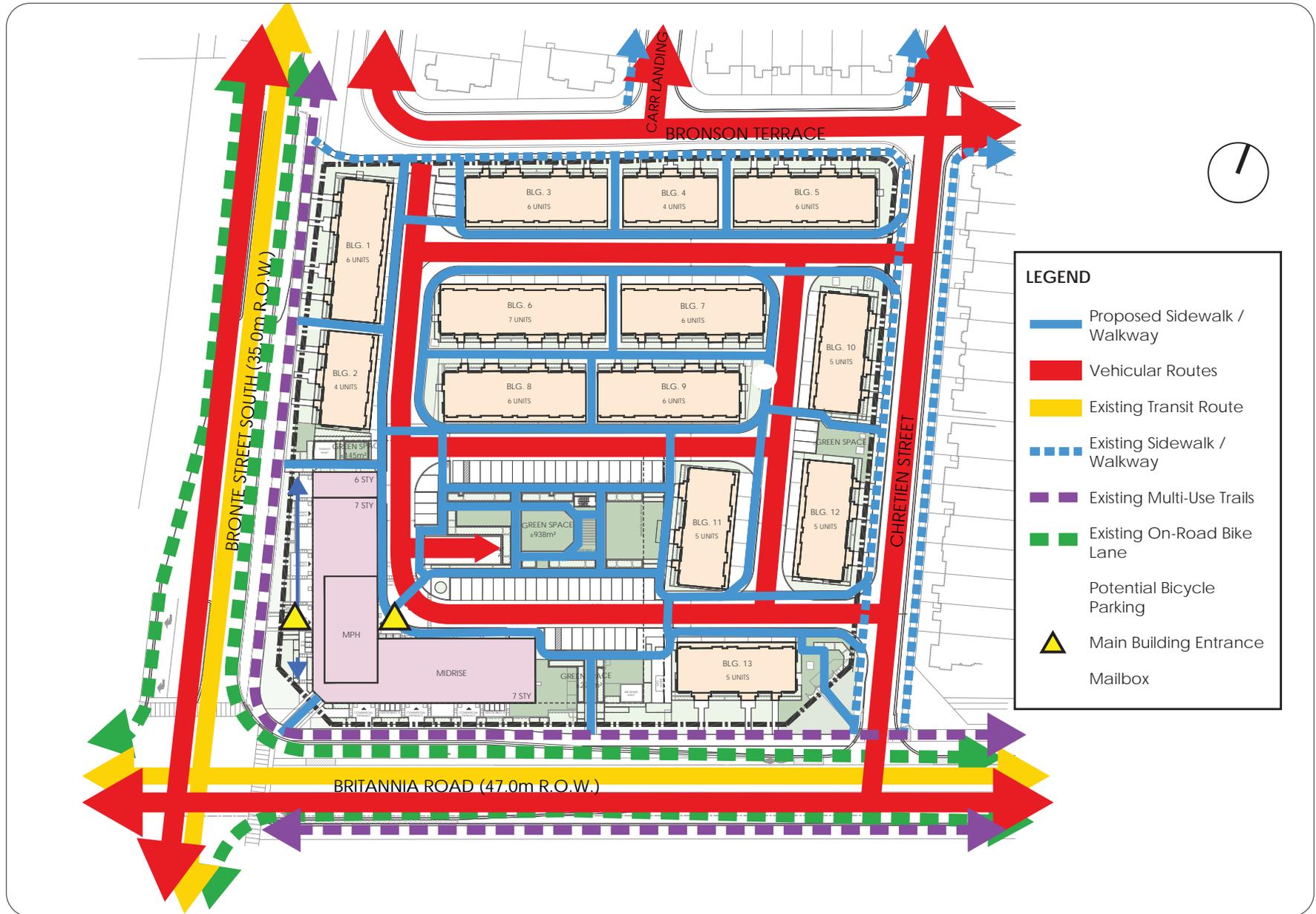
3.2.2 Roads / Sidewalks

- Britannia Road and Bronte Street South are existing arterial roads along the site's west and south boundaries. Vehicular access from these roads is not permitted.
- Bronson Terrace is a local road to the north of the site. Chretien Street is a collector road that forms the eastern boundary.
- Two vehicular access points to the site will occur from Chretien Street via 7.0m private condominium roads with a sidewalk on one side of the road.
- These private roads are intended for local traffic use only and therefore the streetscape should include more pedestrian-scale elements.
- The streetscape should be designed to prioritize pedestrians over vehicles.
- Visitor and commercial parking spaces are provided at grade in proximity to the mid-rise apartment and townhouse buildings. A majority of parking stalls are located around the perimeter of the centralized green space and several pockets throughout the site.



A well-defined streetscape contributes to an attractive and pedestrian friendly public realm





Active Transportation Plan

- Sidewalks will be provided strategically throughout the development and within the public right-of-ways of Bronte Street South, Britannia Road, Bronson Terrace and Chretien Street. These features will contribute to pedestrian safety and comfort.
- Pedestrian linkages between Bronte Street South / Britannia Road into the interior of the development site will occur via the green space areas between the mid-rise apartment and townhouse buildings.
- Walkways will be provided to connect main entrances of units to the sidewalk.



Conceptual image of the pedestrian linkages between mid-rise apartment buildings

3.2.3 Street Trees / Landscaping

- Trees and landscaping will be provided throughout the proposed development to foster an attractive landscape appearance. Landscaped areas include:
 - front yards, flankage yards, areas adjacent to visitor parking or underground parking ramp;
 - Bronte Street South, Britannia Road, Bronson Terrace and Chretien Street streetscapes;
 - yards adjacent to the apartment buildings;
 - along development edges.

- Street trees will be located throughout the development in accordance with Town tree spacing requirements and ensuring no conflicts with street furniture, utilities or paved areas.
- Species selection should be of a native shade tree variety however, ornamental or columnar species may be considered for special locations. Shorter canopy height species should also be considered when planting in close proximity to streetlights and other overhead utilities.
- The locations of and specifications for street trees and landscaping will be detailed on the Landscape Plans for the development.



Conceptual image of streetscape landscape treatments

3.2.4 Outdoor Open Spaces

- The primary outdoor green space is located in the central portion of the development easily accessible by all residents. This passive outdoor green space will provide an attractive focal point within the proposed development and serve as key social gathering space for use by local area residents. Furthermore, this central outdoor green space area together with the outdoor green spaces between the mid-rise building and townhouse buildings 2 and 13 will form a view corridor to overlook / preserve views of the Escarpment.
- The outdoor green space will cater to all age groups and may include the following elements: paved pedestrian walkways, decorative paving surfaces, shade structures and seating/ dining areas, site furniture, open lawn, a play area, ornamental planting, and pedestrian-scaled lighting. Refer to the Landscape Concept Plan on pages 29 and 30 for details.
- The outdoor green space will front onto proposed private roads/ visitor



Conceptual images of the centralized outdoor open space



Conceptual images of interface between the outdoor open space and townhouse units



parking areas and townhouse units of Building 11. The private and public spaces will be separated through the use of planter beds, together with landscape elements such as shrubs, flowers and trees.

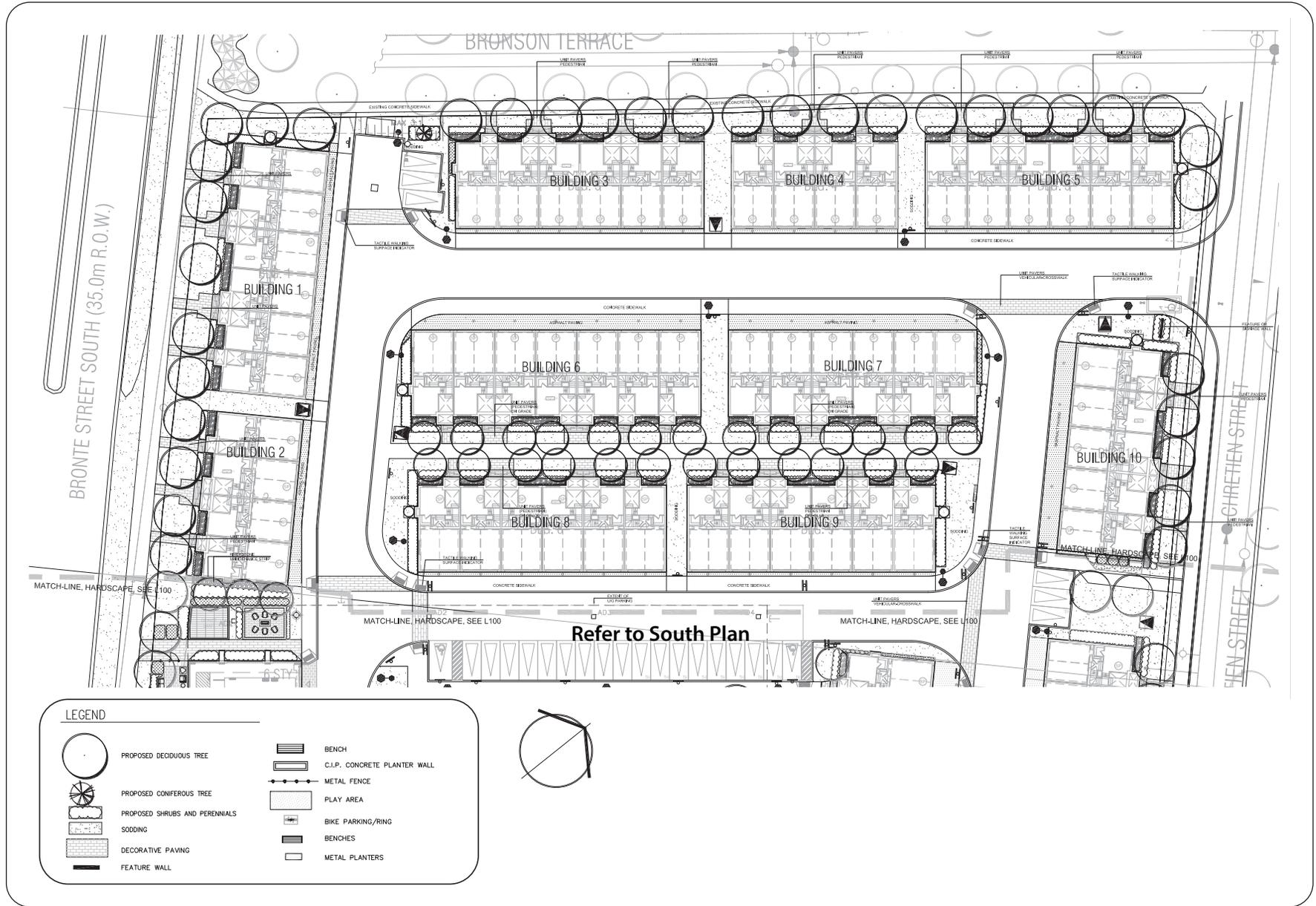
- Townhouses that overlook the central outdoor green space shall have well-articulated front facade treatments with ample glazing and well-defined entrances to serve as an attractive backdrop to this open space amenity area.
- Additional smaller outdoor green spaces are provided in between the apartment and townhouse buildings and in the form of a landscaped mews between Buildings 6/7 and 8/9, and Buildings 10 and 12. At the ground level, these pocket areas can be used as privately owned public space and may include raised planters, masonry walls and pillars, decorative paving, seating areas, shade structure, ornamental planting, trees and an open lawn, dog run/ pet relief area, as well as a retail spill out area for the commercial uses associated with the apartment building. Landscaping and fence treatments should be designed to maximize natural surveillance of the various outdoor open space areas.

3.2.5 Community Mailboxes

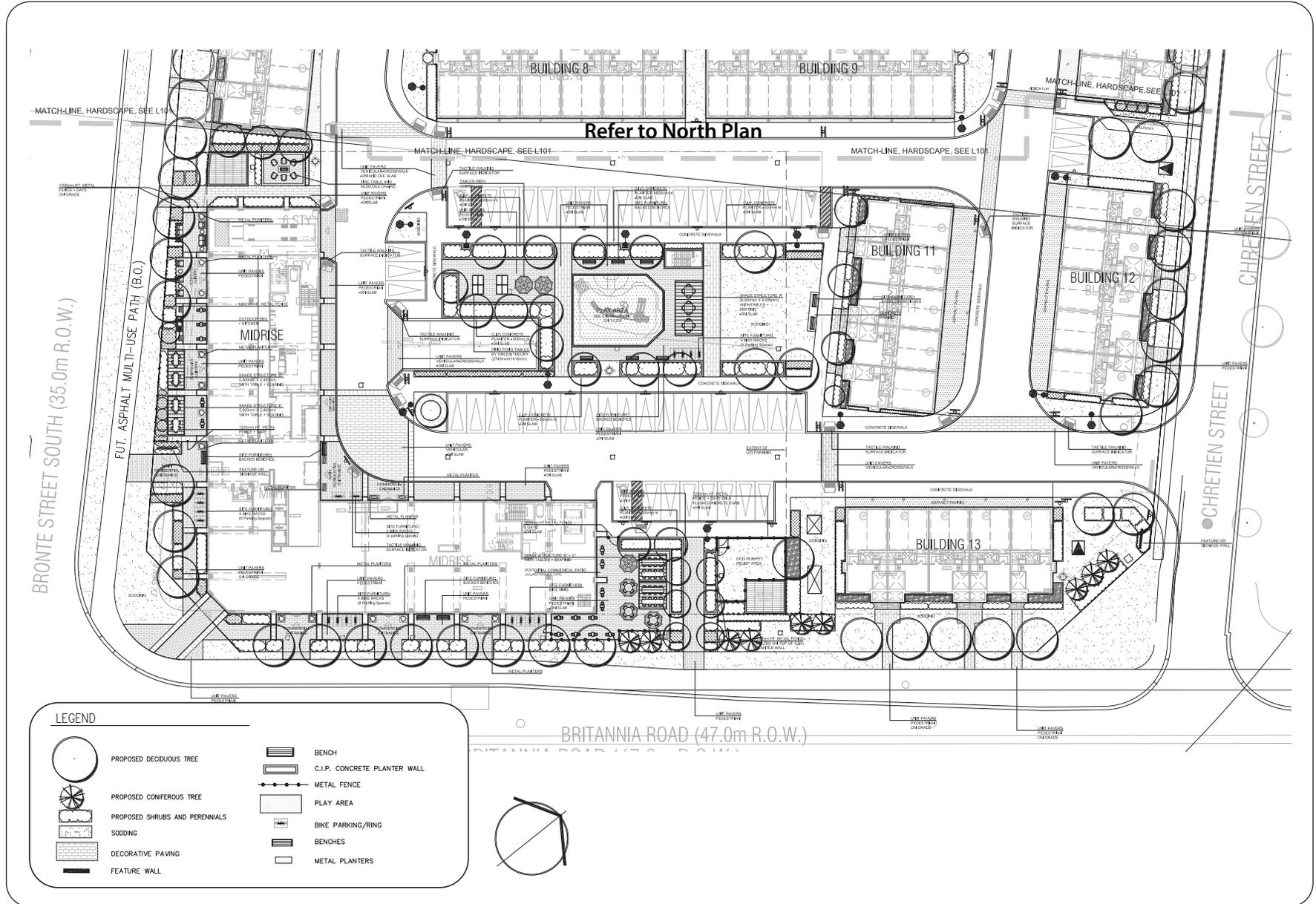
- Community Mailboxes for townhomes will be located in a public space that is easily accessible. This location is safe and visible while protecting the privacy to the adjacent residents.
- Indoor mailbox rooms will be located near the lobby of the mixed use and residential apartment buildings.
- The location of community mailboxes will be detailed on the Engineering Drawings and Landscape Plans for the development.



Conceptual image of community mailbox



Landscape Concept Plan - North Plan (Prepared by Strybos Barron King Landscape Architecture)



Landscape Concept Plan - South Plan (Prepared by Strybos Barron King Landscape Architecture)

3.2.6 Screening / Fencing

- The design of fences and screens should be cohesive throughout the development and should complement the established fence design currently used within the Main Sail Estates subdivision. However, with the proposed development utilizing modern architecture and the existing subdivision displaying neo-traditional architecture, some flexibility should be permitted.
- Visually permeable fencing may be considered for areas within the streetscape where it is desirable to define public space from private space.
- The location of and specifications for fencing will be detailed on the Landscape Plans for the development.



Conceptual image of rear yard privacy screens between adjacent units



Conceptual image of street lighting

3.2.7 Lighting

- High quality outdoor lighting should be integrated into the building architecture and located strategically throughout the site to ensure nighttime safety, security and enjoyment while preserving the ambiance of the night.
- Pedestrian routes should be well-lit to promote pedestrian safety and use of public spaces.
- Outdoor lighting shall be selected and located to reduce light pollution and avoid light spillage or glare on nearby properties and those living in the building above.
- Outdoor site and building lighting should be task oriented and not excessive. Use of full cut-off light fixtures that cast little or no light upward in public areas is encouraged.
- Energy efficient lighting should be utilized to conserve resources.
- The location and design of street and pedestrian lighting will be detailed on the Engineering Drawings and Landscape Plans for the development.

3.2.8 Utilities

- Ventilation shafts, vents, utility meters, and other above-ground mechanical equipment or site servicing elements should be located away from public sidewalks and outdoor open spaces or screened to minimize negative impact.
- Landscape screening of utilities is not preferred, except for transformers.
- Where possible, utilities should be located in an accessible unit or centre that is evenly distributed within the development.
- For townhouse forms, utility meters should either be ganged together and architecturally screened or recessed into a niche for the individual dwelling unit such that they are not highly visible within the public realm. Placement of meters shall comply with local utility company requirements.
- For the mid-rise building, rooftop mechanical and telecommunications equipment shall be concealed from public view and integrated into the design of the building. This may also include acoustic screening where necessary, using materials to match the building.

- The location of surface utilities will be detailed on the Engineering Drawings and Landscape Plans for the site plan.

3.2.9 Loading, Service and Garbage Areas

- The ramp to the underground parking garage shall be located centrally and conveniently.
- Delivery, servicing and garbage collection shall be located in an area that can be conveniently accessed by service vehicles.
- Loading, garbage, staging and servicing areas will be integrated into the building design or screened with landscaping, walls or fencing to minimize negative impacts on adjacent properties.
- Delivery, servicing and garbage collection for the mid-rise buildings shall be located in an area that can be conveniently accessed by service vehicles in the northwest portion of the site. Garbage bins will be stored internal to the buildings and will be brought to an outside staging area on garbage pick-up day.
- Noise attenuation measures shall be provided where service areas are in proximity to sensitive land uses. These features should be complementary in material and design to surrounding buildings / structures.
- Rooftop mechanical and telecommunications equipment shall be screened from public view and integrated into the design of the building.
- A swept path analysis will be prepared by others to ensure adequate space for service vehicle movements is provided.

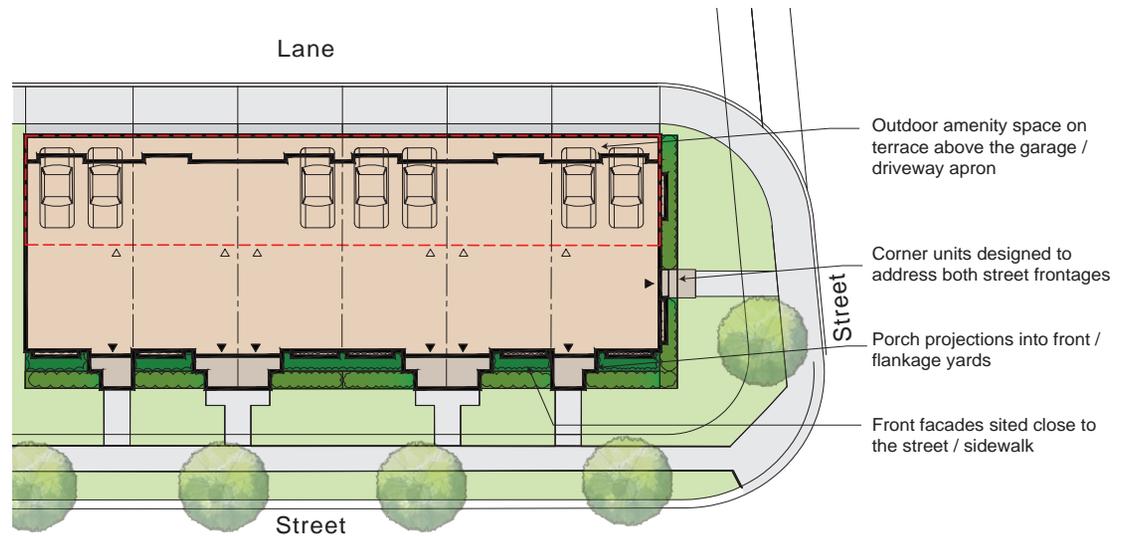


3.3 BUILT FORM

3.3.1 Double Car Garage Townhouses

Double Car Garage Townhouses (common element condominium) are located along portions of Bronte Street South, Britannia Road, Bronson Terrace, Chretien Street, and within the internal development (Buildings 1 - 13). This transit-supportive housing forms contributes positively to the built form character and urban streetscape appearance of the neighbourhood by concealing garages and driveways from the public realm and establishing a strong uninterrupted street edge that is more urban in character. The following design criteria will apply:

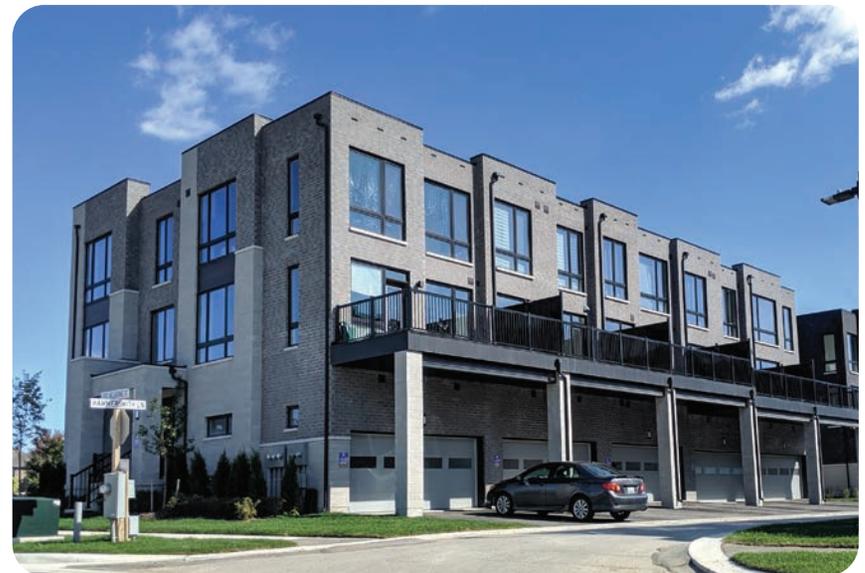
- Block sizes will vary between 4 to 7 units.
- The main front façades and entrances of Buildings 1 and 2 shall face Bronte Street South, Buildings 3



Conceptual Demonstration Site Plan for Double Car Garage Townhouses



Front Elevation



Rear Elevation

Conceptual Images of 3-Storey Double Car Garage Townhouse Dwellings



- 5 will face Bronson Terrace, Buildings 10 and 12 will face Chretien Street, Building 13 will face Britannia Road, Building 11 will be facing the central outdoor green space, and Buildings 6 - 9 will face onto a landscaped mews.

- Both front and rear elevations of the dwelling shall have similar architectural detailing to ensure an attractive and consistent streetscape appearance is achieved.
- All units will have 3 storey building massing plus a rooftop access.
- The use of traditional architecture will be provided.
- The proposed architectural character and materials selected should be complementary with the other buildings (apartment building) to create a visually cohesive development.
- All units will have a two-car garage.
- The garage will be accessed from the rear of the unit via the private laneway and shall be integrated into the main massing of the building.
- Outdoor amenity areas will be provided for each dwelling and may take the form of a balcony or raised terrace located above the garage.
- Double car garage townhomes shall have a walkway linking the front door to the sidewalk within the public or private road together with a landscaped edge to establish an attractive and active streetscape.



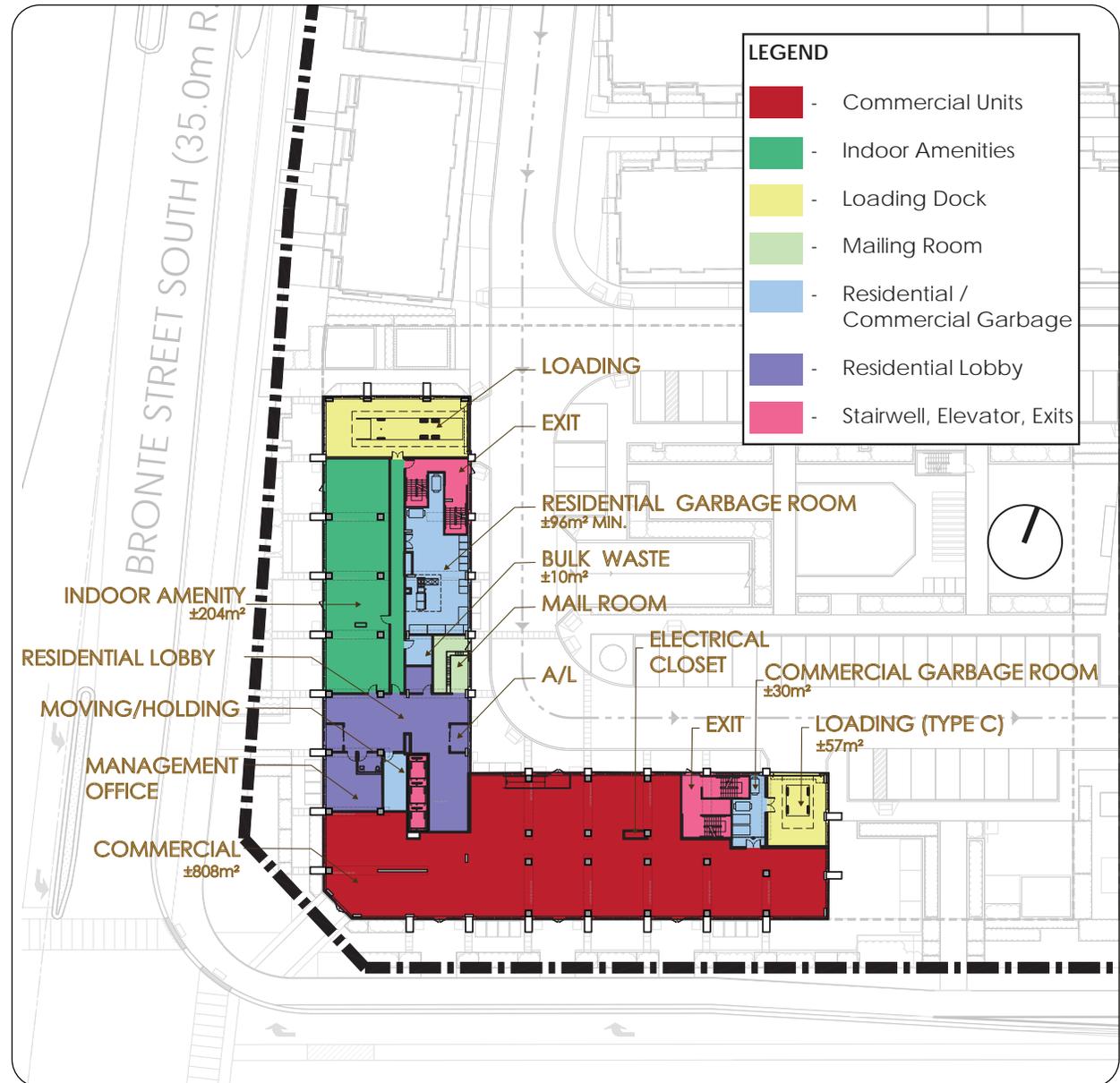
Conceptual Double Car Garage Townhouse Renderings (Prepared by One Riser Designs)



3.3.2 Mixed Use Mid-Rise Apartment Building

The proposed mixed use mid-rise apartment building has been designed to wrap the north-east corner of Bronte Street South and Britannia Road and provide a focal buildings within this Major Node. The intent is to create a compact, higher density built form with a high quality urban identity that will promote an attractive, pedestrian-oriented streetscape and complement the evolving character of the local area. Excellence of building design shall be exhibited to assist with the Town's place-making goals and the design and site layout shall comply with the Town of Milton Mid-Rise Guidelines (May 2018).

The proposed condominium building is designed to anchor the site of the mixed use node. Clean, contemporary lines, used in combination with high quality materials, integrates the buildings with the surrounding community while still creating a unique addition to the architectural milieu of the area. The mid-rise building is located along the intersection of Bronte Street South and Britannia Road. The mid-rise building will add interest to the new urban street edge and help to define a sense of place for this major node area.



Site Demonstration Plan for Apartment Building

The following design criteria will apply:

- The mid-rise building will be 7-storeys, plus mechanical penthouses, at the corner of Bronte Street South and Britannia Road.
- A total of 152 units are proposed in a variety of one bedroom, one bedroom + den, two bedroom and two bedroom + den suites sizes.
- The ground floor level, located at the intersection of Bronte Street South and Britannia Road, will include approximately 808m² (8697.24 sq.ft.) of commercial / retail space.
- In mixed use locations, ground floor heights should be 4.5m to allow for use flexibility and adaptation.
- The tenure of the mixed use mid-rise apartment buildings will be condominium.
- The design of the building provides for appropriate transitions in height and separation with the adjacent buildings and the neighbouring built form (townhouses) to ensure adequate sunlight and sky views are maintained for surrounding streets and buildings.
- A Shadow Impact Study has been undertaken by RBA (Ralph Bouwmeester and Associates) and will be evaluated against the criteria within the Town of Milton's Shadow Impact Analysis Guidelines.
- The proposed mid-rise building and adjacent townhouse buildings are separated by an estimated 10.9m to 36.3m to allow for outdoor open spaces and pedestrian linkages into the centre of the development from Bronte Street S. and Britannia Road.



Wall articulation, materials and colours will support the architectural character and break up horizontal massing of the buildings



The corner of the building will have focal emphasis

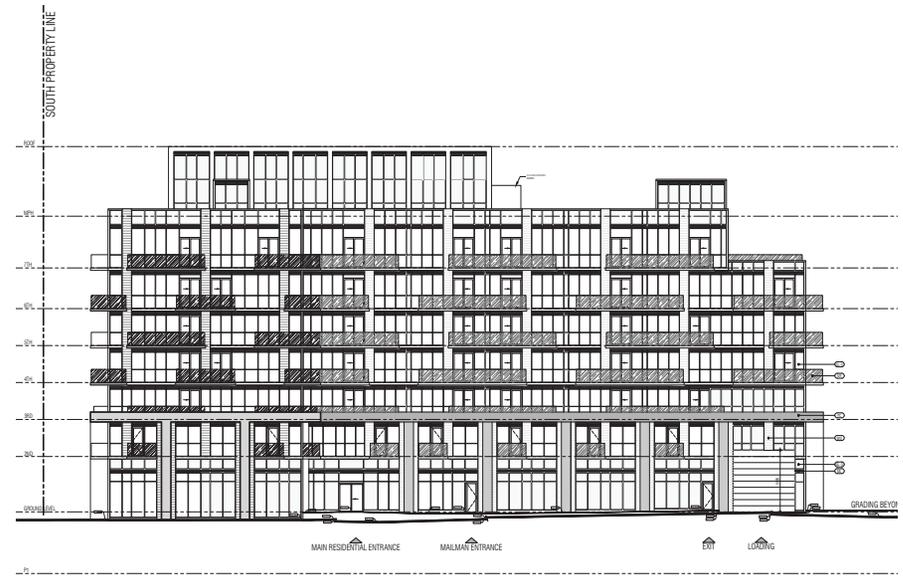


The apartment buildings will be designed and sited to promote an attractive, pedestrian-oriented streetscape with ground level commercial / retail opportunities

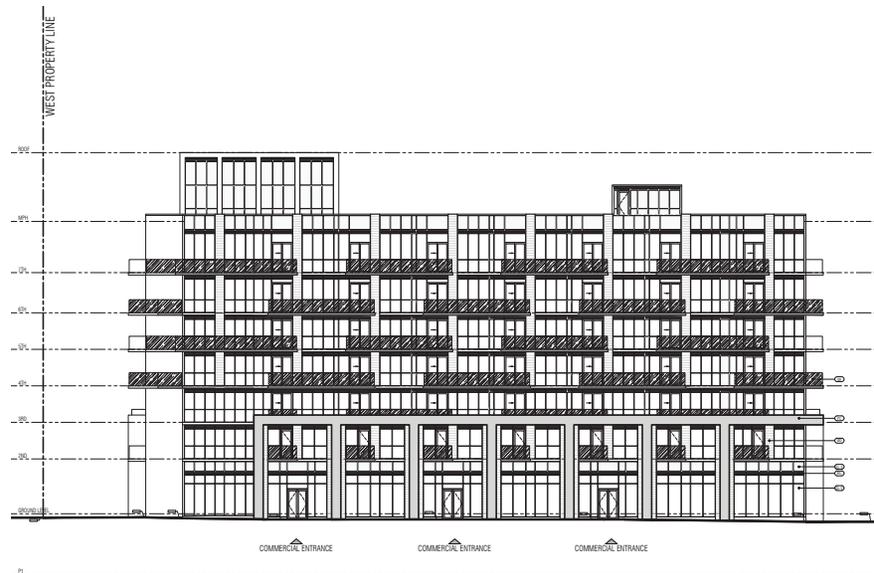




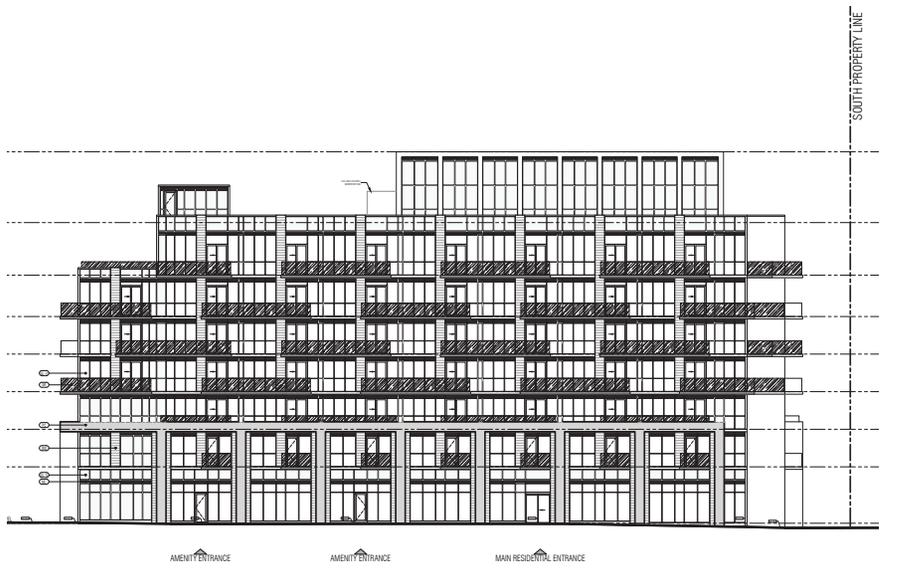
North Elevation



East Elevation



South Elevation



West Elevation

Proposed 12-Storey Mixed Use Mid-Rise Apartment Elevations (Prepared by Graziani + Corazza Architects)

MIDRISE_G.F.A. BREAKDOWN

FLR.	G.C.A. (m2)	G.C.A. (ft2)	T.C.A. (ft2)	EXCLUSIONS (m2)	COMMERCIAL G.F.A. (m2)	RESIDENTIAL G.F.A. (m2)	TOTAL G.F.A.	CORR (ft2)	T.S.A. (ft2)	EFF.%
P1	5705	61408				5705	5705		61408	100%
1ST	1650	17766		17	808	842	1650	9078	8688	49%
2ND	1536	16533		18		1518	1518	2341	14192	86%
3RD	1536	16533		18		1518	1518	2341	14192	86%
4TH	1650	17766		18		1632	1632	2410	15356	86%
5TH	1650	17766		18		1632	1632	2410	15356	86%
6TH	1650	17766		18		1632	1632	2410	15356	86%
7TH	1541	16586		18		1523	1523	2431	14156	85%
MPH	410	4412						4412	0	0%
TOTAL	11215	120714	0	128	808	10296	11104	23419	97295	81%

*UNDERGROUNDS AND MPH NOT INCLUDED IN TOTALS

*COMMERCIAL TOTAL INCLUDES COMMERCIAL GARBAGE AND LOADING SPACE

UNIT MIX MIDRISE

FLOOR	STUDIO	1B	1B+D	2B	2B+D	3B	TOTAL
1ST	0	0	0	0	0	0	0
2ND	0	10	9	5	1	0	25
3RD	0	8	9	5	2	0	24
4TH	0	9	9	5	3	0	26
5TH	0	9	9	5	3	0	26
6TH	0	9	9	5	3	0	26
7TH	0	11	9	4	1	0	25
TOTAL	0	56	54	29	13	0	152

PARKING SPACES

		REQUIRED SPACES				
REQUIRED	MIDRISE	1.00	152			152
	VISITOR	0.20	152			31
	DBL CAR RES.	2.00	72			144
	DBL CAR VISITOR	0.20	72			15
	TOTAL COMMERCIAL/VISITOR					46
	TOTAL RESIDENTIAL					296
TOTAL REQUIRED IN UNDERGROUNDS						342
PROVIDED:						351
	FLOOR	LAYBY /SHORT TRM.	DCL CAR	COMM./VIS.	RESIDENTIAL	TOTAL
	P1				152	152
	GROUND		144	55		199
	CAR SHARE					0
TOTAL						351
EXCESS RESIDENTIAL SPACES IN UG:						9

BF PARKING REQUIRED

		REQUIRED SPACES		
COMMERCIAL	4%	31		2
VISITOR	4%	15		1
RESIDENTIAL	2 PLUS 2%	152		6

*NOTE: 1 CAR SHARE SPACE = 4 PARKING SPACE

*LAYBYS ARE NOT INCLUDED IN TOTAL

*TOTAL OF XX BARRIER FREE SPACES

BIKE PARKING/LOCKER COMBO

BIKE RATIO	# OF UNITS	REQUIRED SPACES	
RESIDENTIAL	1.00	152	152
VISITOR	0.05	152	8
TOTAL			160
PROVIDED:	196		123%
FLOOR		VIS. BIKES	RES. BIKES
P1			98
1ST		44	44
2ND			9
3RD			9
4TH			9
5TH			9
6TH			9
7TH			9
TOTAL		44	152

AMENITY

	# OF UNITS	RATE (4m2/per)	REQUIRED	PROVIDED
OUTDOOR	152	4	608	1362
TOTAL				1362
PROVIDED:				224%
FLOOR		INDOOR m2	OUTDOOR m2	
1ST		204	1362	
TOTAL		204	1362	

Mixed Use Mid-Rise Apartment Statistics Prepared by Graziani + Corazza Architects)





Britannia Road Coloured Elevation (South Elevation) (Prepared by Graziani + Corazza Architects)





Bronte Street Coloured Elevation (West Elevation) (Prepared by Graziani + Corazza Architects)





Conceptual Perspective View of the Mixed Use Mid-Rise Apartment Building as viewed from Britannia Road and Bronte Street South

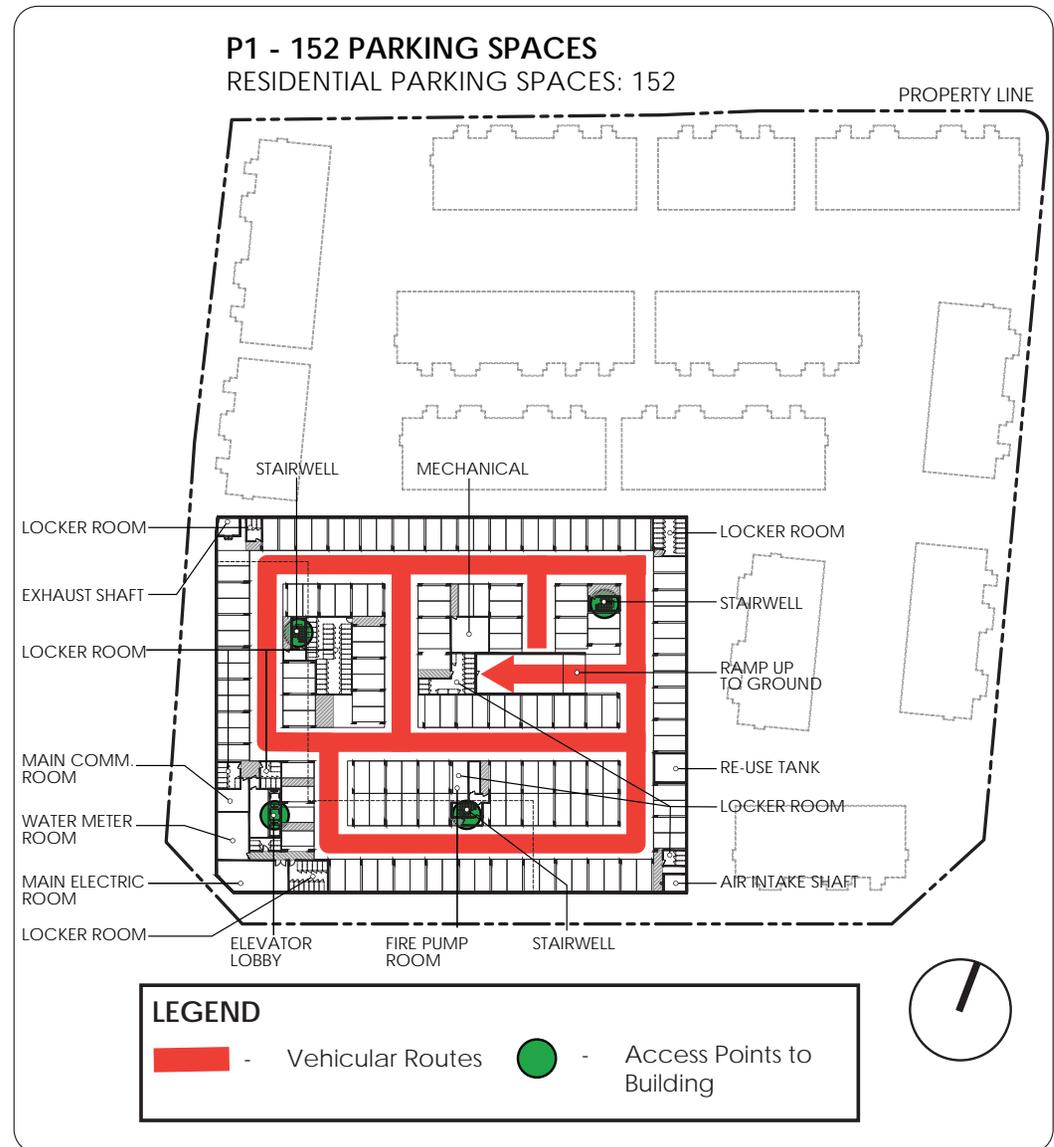




Conceptual Perspective View of Mixed Use Mid-Rise Apartment and Townhouse Buildings as viewed from Britannia Road and Chretien Street



- Each apartment unit will have private outdoor space in the form of balconies and terraces to enhance the living environment of residents. These features should be large enough to comfortably accommodate space for seating. Direct access to ground-level suites from the street is proposed.
- Facades shall be well-articulated and provide ample fenestration facing public areas. A greater proportion of wall openings (windows, doors) to solid will be provided to reinforce a human scale environment, particularly at street level. At least 60% of the street frontage should be active uses.
- Architectural design treatment (wall/roof articulation, doors, fenestration, masonry detailing, character lighting) shall be consistent on all elevations to avoid uninteresting or blank façades.
- Main entrances will be designed as focal points of the building to create a strong sense of entry along Bronte Street South and Britannia Road. Entries, including for commercial/retail uses, will be recessed or covered for weather protection and provide barrier-free access to the buildings.
- The use of a variety of high quality, durable, low-maintenance building materials that support the architectural character of the buildings will be used. Application of main wall cladding materials should be consistent on all elevations of the building. Finished wall materials should extend close to finished grade to minimize exposed concrete foundation walls.
- Preferred cladding materials include brick, stone, metal, glass, in-situ concrete and pre-cast concrete. Stucco, vinyl sliding, plastic, plywood, concrete block, tinted and mirrored glass and metal siding is strongly discouraged.
- The use of contemporary architecture is appropriate and complementary with the proposed townhouse forms within the major node.
- Material changes and architectural detailing which



Underground Parking Plan for Apartment Building

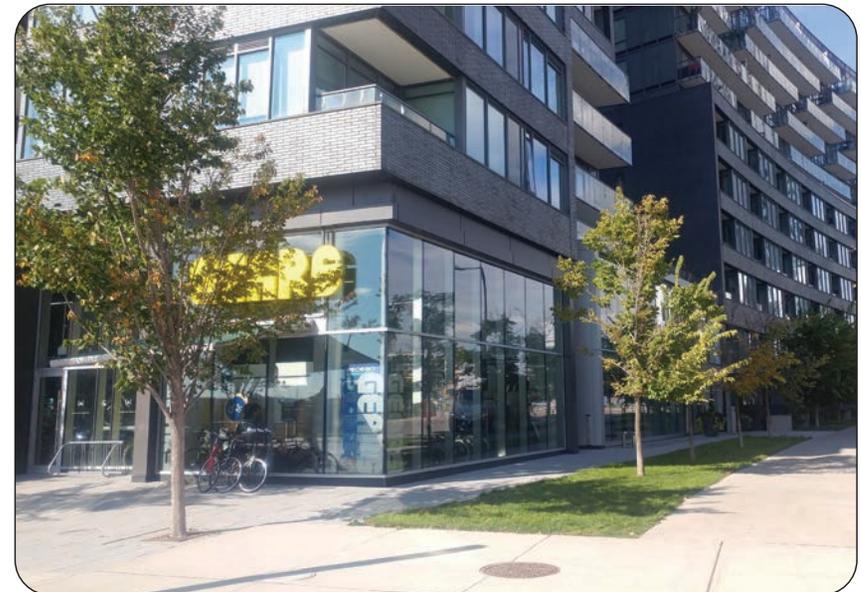
help to articulate the transition between the base, middle and top of the building are appropriate. Where changes in materials occur they should happen at logical locations such as a change in plane.

- Integrate rooftop mechanical and telecommunications equipment into the design of the building so they do not visually detract from the skyline.
- A majority of parking will be provided in an underground parking garage. Driveway access ramp to the underground parking area will be located at the rear of the building in a well-lit, easily identifiable but unobtrusive manner. Ample lighting shall be provided within the parking garage.
- Apartment units will follow the minimum parking space requirements:
 - 1.0 vehicle spaces per residential unit; and,
 - 0.2 vehicle spaces per commercial / visitor.
- A parking rationale study has been prepared by GHD and will accompany the submission.
- Commercial / visitor parking spaces located at the rear of the building, along the private road, are placed away from public street frontages and located close to the building entrances.
- Bicycle parking has been provided at the rate of 1.0 spaces per residential unit. These are provided within the underground parking garage in secure lockers. Visitor bicycle spaces are located aboveground near the main entrance to the building and have been provided at a rate of 0.29 spaces per unit.
- Service, garbage/recycling and moving/holding areas are provided for the building and will be located away from Bronte Street South and Britannia Road. Space for these elements will be integrated into the building design to minimize negative impacts of noise, visibility, odours and vibrations on adjacent properties. Two internal loading space has been provided in the north and east corners of the building. Refer to Site Demonstration Plan on page 35. A swept path analysis has been prepared by GHD to ensure that adequate space is provided for service vehicle movements and will accompany the submission.

3.3.2.1 Street Interface

The interface between new mid-rise development within Major Node Block 360 and the adjacent street frontages of Bronte Street South and Britannia Road shall be carefully considered, including:

- Maximum and minimum street wall heights should be proportionate to adjacent road R.O.W. width to create a sense of enclosure and provide a comfortable pedestrian zone.
- The street wall facade shall include articulation, fenestration and plantings to create an attractive environment for pedestrians.
- Building design should consider material change above the building base or podium. A podium height of 2-4 storeys highlighted by architectural elements such as entrances, canopies, large areas of glazing and retail opportunities, will encourage a pedestrian-scaled streetscape.



Example of Street Interface for Mid-Rise Mixed Use Building

3.3.2.2 Transition to Neighbourhood Context

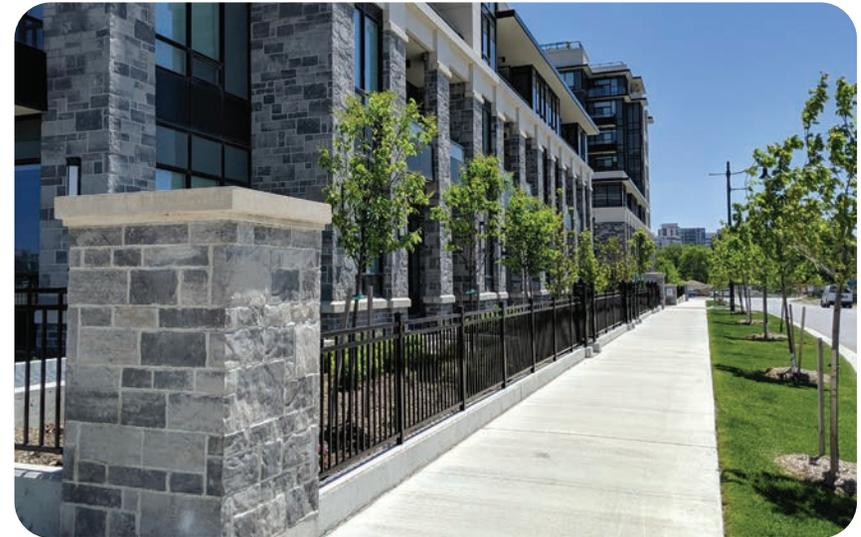
New mid-rise development shall be designed to provide a sensitive and gentle transition in scale to the adjacent low-rise neighbourhood, including:

- Building massing should be highest close to the Bronte Street South/Britannia Road intersection to provide greater architectural emphasis.
- Building heights should transition down through use of Double Car Garage Townhouses where in proximity to existing low-rise residential forms.

3.3.2.3 Open Space and Parking

The design of open space features and parking areas within Major Node Block 360 shall combine to create a livable and pedestrian friendly environment, including:

- Main parking areas will be located below ground.
- Surface parking areas should generally be limited to barrier-free parking, visitor parking and drop-off zones for loading/unloading.
- Additional building setbacks at the corner of the Bronte Street South/Britannia Road intersection should be applied to create an open space gateway feature.
- Common outdoor green spaces that provide for passive and active recreation will be located within Major Node Block 360 for the benefit of local residents.
- Mid-block pedestrian connections, together with linkages to adjacent trails, sidewalks and transit stops shall be provided to maximize permeability through the site and to promote active transportation opportunities.
- The amount of landscaping should be proportionate to the overall parking lot size, but generally, 1 tree for every 8 parking spaces is recommended.
- Preferential parking for bicycles, energy efficient vehicles and car-share services are encouraged.



Example of Street Interface for Mid-Rise Residential Building



Building heights should transition down to low-rise forms through use of setbacks / terracing



3.4 SUSTAINABILITY

Sustainable development practices balance the health and wellbeing of the environment and related resources with the pressure of urbanization, bringing forward strategies to better manage increased population densities, resource and energy consumption and vehicular traffic volumes. A variety of Low Impact Development (LID) and sustainability design initiatives will be considered in the development of the land and the construction of new buildings. These features are intended to utilize energy efficiency and protect and conserve water in order to promote a healthy and sustainable neighbourhood.

3.4.1 Development Considerations

The environmental sustainability principles addressing water quality, consumption and runoff are key issues. A range of appropriate design measures will be considered, such as reduction of hard surfaces and addition of landscaping. The various elements proposed will be determined in conjunction with the Town and may include:

- Provision of bio-swales and/or infiltration trenches.
- Additional depth topsoil placement.
- Use of energy efficient LED lighting.
- Provision of a connected pedestrian system.
- Integration with future transit routes.
- Providing trees and landscaping that increases the urban canopy, creates comfortable micro-climate conditions and mitigates negative seasonal effects (wind breaks or shade canopy).
- Resource management measures during construction, including:
 - A waste management policy to ensure that all trades work efficiently to reduce and eliminate waste.
 - Erosion control filter cloth measures on all catch basins.
 - Dedicated on-site concrete washout areas.
 - Indoor contaminant control during construction.
 - Purchasing stone, concrete, masonry from regional/local sources.

3.4.2 Building Considerations

MIXED USE / RESIDENTIAL APARTMENT BUILDING

- The nature of the mixed use apartment building encourages a healthy, pedestrian-oriented lifestyle and provides an alternative to urban sprawl that is typically defined by low density housing forms. A smaller footprint will be utilized to house a larger number of people than a typical low density neighbourhood, providing a more efficient and sustainable housing form and also provides for commercial/ retail opportunities.
- The main aspects of site and building design to be addressed with an emphasis on sustainability include improved air quality, reduced urban heat island effect, energy efficiency, improved water quality and efficiency, and reduced light pollution.
- A range of unit size choices will be offered to satisfy the varied needs of current and future uses and allow sufficient flexibility for future change over time.
- Renewable and high efficiency energy systems should be utilized.
- Use of sustainable, local and renewable materials, high efficiency appliances and low-VOC materials and finishes will be considered. Day-lighting, double glazed windows and overhangs for shade in summer will be employed to reduce energy, heating and cooling loads.
- Lighting will be designed in a manner that minimizes light pollution and helps maintain a dark night sky. Site lighting will be sufficient for pedestrian safety. Timers and sensors should be considered for indoor lighting.
- Reflective white roofs may be considered to reduce the heat island effect.
- New building should be encouraged to seek Leadership in Energy and Environmental Design (LEED) Certification, or an equivalent design standard.

LOW-RISE RESIDENTIAL BUILDINGS

- EnergyGuide 83 will be used as a base standard for new townhome construction.



- High-efficiency single- or dual-flush toilets and low-flow water efficient faucets / shower heads.
- High-Efficiency HVAC system.
- Energy Star qualified standard light fixtures.
- Energy Star windows throughout.
- Hot Water Drain recovery system.
- Energy-Efficiency upgrade offerings including electronic HEPA filter systems, panel humidifiers, ERV's, Higher SEER A/C Units.
- Solar power and Electric vehicle rough-ins.
- Energy efficient hot water systems.

3.4.3 Community Safety

In order to promote a safe, pedestrian-friendly community, the design and siting of all new buildings will respect CPTED (Crime Prevention Through Environmental Design) principles, including the following:

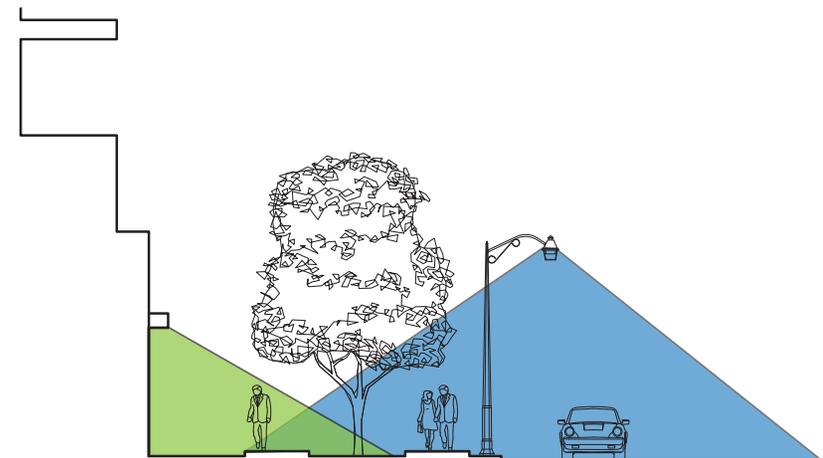
- A clear definition between public and private space will be provided through the design and placement of buildings, fencing and landscaping.
- Site planning and building design will allow for visual overlook of public spaces.
- Ample fenestration facing public areas will be provided to promote natural surveillance (eyes on the street) from within the building.
- Balconies, terraces, porches or other similar outdoor features will be provided to promote natural surveillance and serve as an interface between private and public realms.
- Main entrances to the buildings will be visible from the street and clearly defined.
- Blank and inactive building façades adjacent to public areas, pedestrian walkways and gathering places will be avoided.
- Lighting will be designed to relate to the pedestrian scale and illuminate all pedestrian routes. Parking areas, sidewalks, driveways and walkways will be adequately illuminated with low level, pedestrian-scaled lighting. Site lighting will be directed downward and inward to mitigate negative

impact on neighbouring uses.

- The presence of parking lots and garages within the streetscape will be diminished by locating main parking areas underground for the apartment building and providing integrated attached garages for low-rise uses. Where garages face the street they shall have limitations to their



Streets, parking areas, sidewalks and walkways should be adequately illuminated.



Building Design / Placement, Lighting and Fenestration Shall Combine to Provide a Safe Pedestrian Environment

width and should not project beyond the building's front wall or porch face.

- All building and garage entries shall be well lit.
- To ensure safety within the private road network the following measures should be considered:
 - All private roads shall be adequately lit with pedestrian scaled light standards;
 - Address signage shall be placed in a well lit location on the building.
 - Public overlook of private roads will occur through the use of outdoor amenity space and/or fenestration.
 - The length of private roads will be kept short.

3.4.4 Open Space

- In addition to the common outdoor open spaces proposed within Major Node Block 360, the site will also have access to the existing Village Square situated east of the study area, in a centralized location within the Main Sail Estates Subdivision. The Village Square is in an area that is physically and visually accessible via the proposed road and sidewalk networks (Bronson Terrace). The Village Square will provide an amenity / gathering space for the neighbourhood that will support social interaction and passive recreation.

3.5 IMPLEMENTATION

- Development of the subject lands will be realized through Zoning by-Law Amendment and a Site Plan Approval process to be administered by the Town of Milton.
 - Detailed site plans, landscape and architectural drawings, and shadow study based upon the concepts and design guidelines within this Urban Design Brief & Architectural Control Guidelines, shall be submitted as part of the Site Plan application.
 - The guidelines contained within this UDB & ACG and their interpretation by Town staff are not intended to discourage design creativity or innovation. Proposed designs that are not in total compliance with the guidelines may be considered by the Town, based on their merits, and may be approved where the spirit and intent of the guidelines is preserved.



The Village Square will provide an amenity / gathering space for the neighbourhood

