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# **PLANNING JUSTIFICATION REPORT**

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**APPLICATIONS FOR  
OFFICIAL PLAN AMENDMENT  
ZONING BY-LAW AMENDMENT  
DRAFT PLAN OF SUBDIVISION**

6728 Sixth Line  
Part of Lot 10, Concession 6, New Survey (Trafalgar)  
Town of Milton  
Anatolia Real Estate Corp  
January 2024  
GSAI File # 1115-004

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**Planning Justification Report  
Official Plan Amendment  
Zoning By-law Amendment  
Draft Plan of Subdivision**

**6728 Sixth Line, Town of Milton, ON**

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## **1.0 Introduction**

Glen Schnarr & Associates Inc. has been retained by Anatolia Real Estate Corp. to compile this Planning Justification Report in support of applications for an Official Plan Amendment (OPA), Zoning By-law Amendment (ZBA), and Draft Plan of Subdivision. These applications facilitate the development of an industrial subdivision on the property situated at 6728 Sixth Line, legally referred to as Part of Lot 10, Concession 6, New Survey (Trafalgar).

The core objective of this Planning Justification Report is to delineate the nature of the proposed OPA, ZBA, and Draft Plan of Subdivision and to assess these proposals within the framework of relevant policies. The assessment specifically considers compliance with the Provincial Policy Statement, the Provincial Growth Plan, the Region of Halton Official Plan, the Town of Milton Official Plan, and the Derry Green Corporate Business Park (DGCBP) Secondary Plan. This report serves as a critical step in the process of obtaining the necessary approvals for the development, ensuring alignment with regional, provincial, and local planning guidelines.

## **2.0 Site Description and Surrounding Land Use**

The subject lands are situated to the south of Derry Road and west of Sixth Line, with a smaller section extending to the east of Sixth Line where the street diverts due to the presence of Sixteen Mile Creek, as visually depicted in **Figure 1 – Aerial Context Plan**. Historically, the site served as the location of the former Trafalgar Golf & Country Club, featuring four associated buildings and structures. Tributaries of Sixteen Mile Creek traverse the site from the north and west, converging in the southeast corner. The smaller parcel east of Sixth Line is currently undeveloped, with dense vegetation within the floodplain of Sixteen Mile Creek.

The overall configuration of the site is predominantly rectangular and spans approximately 62 hectares (153 acres) in size. It boasts 930 metres (3,051 feet) of frontage along Derry Road (Regional Road 7) and 610 metres (2,001 feet) along Sixth Line. The site's topography gradually descends in grade as one moves towards the rear (south) of the property. Derry Road (Regional Road 7) is classified as a Major Arterial Road with a planned right-of-way (ROW) of 42 metres (138 feet). Access to the site is currently facilitated via Sixth Line, which functions as a collector road and features a planned ROW of 30.0 metres (98.4 feet).

### **2.1 Surrounding Land Use Context**

The subject lands are situated within the Derry Green Corporate Business Park Secondary Plan (DGCBP) and are designated as Provincially Significant Employment Zone 18, indicating its importance for employment and industrial activities.

As illustrated in **Figure 1** – Aerial Context Plan, the subject lands are bordered by:

- **North:** Derry Road, areas designated for future development and employment lands, a commuter and commercial rail corridor housing the Canadian Pacific (CP) intermodal yard (with a planned GO station in the future), and Highway 401.
- **South:** The Union Gas pipeline running from east to west, an Institutional Use area (Science of the Soul Study Center/Radha Soami Society Beas Canada), a future industrial Subdivision (Remington Group), and tributaries of the Sixteen Mile Creek.
- **East:** Sixth Line, Sixteen Mile Creek, single detached dwellings, agricultural areas, and Trafalgar Road (providing access to Hwy 401 and the CP intermodal yard), with Highway 407 further to the east.
- **West:** Existing agricultural lands (future industrial Subdivision by Remington Group), a woodlot/Natural Heritage System (NHS), Fifth Line (a future major arterial road), and James Snow Parkway beyond, which provides access to Highway 401.

These lands have the advantage of being located near both Highway 401 and 407 and are easily accessible via major arterial roads in the area. The 21 and 27 GO Bus routes also operate along Derry Road, providing access to inter-regional transit services. Additionally, there are plans for a future GO Station to be built in the northeast, at the intersection of Trafalgar Road and the CP railway. This development is expected to have access to future local transit, making it easy to connect to nearby residential areas.

### **3.0 Proposed Development**

Anatolia is proposing to transform the property into an industrial Plan of Subdivision, which will feature three business park blocks, one commercial block, two stormwater management (SWM) pond blocks, one stormwater management pond buffer block, three Natural Heritage System (NHS) channel/floodway blocks, five Natural Heritage System Buffer blocks and a public street to be known as Clark Boulevard, as indicated in **Figure 2** – Draft Plan of Subdivision.

**Figure 3** – Conceptual Site Plan for the industrial development shows that each of the three business park blocks will house industrial warehouse buildings with accompanying subordinate office spaces, totaling roughly 184,000 square metres of industrial Gross Floor Area (GFA). Office spaces are primarily situated along the Derry Road frontage, with limited parking between the buildings and Derry Road allocated for these offices. Main parking and loading facilities for the warehouses are planned along the side and rear lot lines. It is essential to note that the current design of these blocks is in the conceptual stage and will be refined through the Site Plan Approval process.

On the southeast corner of Derry Road and Clark Boulevard, there is a proposed commercial/retail block. The aim is to have this block complement and support the industrial subdivision while also providing diversified employment opportunities. This is in line with the "complete community" principle that seeks to provide accessible shopping and eating options for adjacent employment uses. Block 2 will share access with this block to Clark Boulevard. Anatolia, the developer, does not currently have plans to develop the commercial/retail block. Instead, they plan to make provisions for it to be developed in the future. A high-level concept design for this block is available in **Figure 11**.

To support the development of the industrial subdivision and ensure that the blocks are of regular shape, the tributaries of Sixteen Mile Creek will be redirected across the property. The channel blocks will be of varying widths to ensure the continued functionality of the creek's tributaries and will include a buffer as per the approved Subwatershed Impact Study (SIS) for the Secondary Plan area. The channel/floodway NHS block is designed strategically to capture storm runoff and overflow, thus avoiding the risk of potential flooding. This is achieved using a combination of stormwater management ponds, detention basins, and underground storage cisterns.

Access to the site for Blocks/Buildings 1 & 2 will be directly from Street A (to be known as Clark Boulevard). This extension will connect to Derry Road (a regional road) and continue south through the planned Remington industrial subdivision before turning west to Fifth Line. Blocks/Buildings 1 & 2 will each have two access points from Clark Boulevard. Block/Building 3 will receive access from Sixth Line. The precise location of this driveway will be confirmed through future Site Plan Applications.

The development of Clark Boulevard will be coordinated with the adjacent landowner, The Remington Group, through a concurrent Draft Plan of Subdivision.

Given that parking is not permitted within the "Street Oriented Overlay" area along Derry Road, an Official Plan Amendment (OPA) is necessary for the Derry Green Corporate Business Park (DGCBP) Secondary Plan to allow parking between the warehouse buildings and Derry Road and for the standalone commercial/retail use to serve the future employees and retail customers. Additionally, a Zoning By-law Amendment (ZBA) is required to rezone the lands to accommodate the proposed development and implement the DGCBP Secondary Plan. Furthermore, a Draft Plan of Subdivision is essential to facilitate the division of land for this comprehensive project.

Located on the southwest corner of Derry Road and Sixth Line is a residential property that is currently not included in the current development plan. The owners of the property have chosen not to participate in the development application. However, it is important to consider the future development of this land and ensure that it is not isolated from the rest of the project. Our office has prepared a high-level concept of how the land can be incorporated into the broader development, which is illustrated in **Figure 12**. Access to the property will be from the internal road network of Block 3 in the proposed draft plan of subdivision.

#### **4.0 Policy and Regulatory Context**

This section of the report will address how the application conforms to the Provincial Policy Statement, the Provincial Growth Plan, the Region of Halton Official Plan, the Town of Milton Official Plan, and the DGCBP Secondary Plan.

The proposed development concept will be evaluated with the required Zoning By-law Amendment against relevant policy frameworks.

#### **4.1 Planning Act, R.S.O. 1990 c.P.13**

Section 2 of the Planning Act requires decision-makers, in carrying out their responsibilities under the Planning Act, to have regard to, among other matters, matters of provincial interest. The following matters of provincial interest are relevant to the evaluation of the proposed development:

*(f) the adequate provision and efficient use of communication, transportation, sewage and water services and waste management systems;*

*(h) the orderly development of safe and healthy communities;*

*(h.1) the accessibility for persons with disabilities to all facilities, services and matters to which this Act applies;*

*(j) the adequate provision of a full range of housing, including affordable housing;*

*(p) the appropriate location of growth and development;*

*(q) the promotion of development that is designed to be sustainable, to support public transit and to be oriented to pedestrians;*

*(r) the promotion of built form that,*

*(i) is well-designed,*

*(ii) encourages a sense of place, and*

*(iii) provides for public spaces that are of high quality, safe, accessible, attractive and vibrant;*

*(s) the mitigation of greenhouse gas emissions and adaptation to a changing climate. 1994, c. 23, s. 5; 1996, c. 4, s. 2; 2001, c. 32, s. 31 (1); 2006, c. 23, s. 3; 2011, c. 6, Sched. 2, s. 1; 2015, c. 26, s. 12; 2017, c. 10, Sched. 4, s. 11 (1); 2017, c. 23, Sched. 5, s. 80.*

After reviewing Section 2 a) of the Planning Act, it is our opinion that the proposed development is consistent with matters of Provincial interest mentioned in this section. The Planning Justification Report describes the specific applications that are not affected by any other matters of Provincial interest identified in Section 2 of the Planning Act. Therefore, we believe that the proposed change of use takes into consideration the matters of Provincial interest.

#### **4.2 Provincial Policy Statement (2020)**

The Provincial Policy Statement (PPS) came into force and effect on May 1, 2020 and was approved under the authority of Section 3 of the Planning Act. Section 3 of the Planning Act requires planning authorities to be consistent with the PPS when exercising any authority that affects planning matters.

As outlined in the PPS, Ontario's long-term prosperity, environmental health, and social well-being depend on a coordinated approach to managing change and promoting efficient land use and development patterns that help build strong and healthy communities. This is achieved by prioritizing environmental protection, public health and safety, economic growth, and the creation of livable, healthy, and complete communities. It is important to read the PPS in its entirety, while considering all applicable policies and how they work together.

The PPS provides policy direction on matters of provincial interest related to land use planning and development. The PPS aims to permit appropriate development while ensuring that resources of provincial

interest, public health and safety, and the quality of the natural environment are protected. All decisions affecting planning matters “*shall be consistent with*” the policy statements issued under the *Planning Act*.

PPS policies applicable to the proposal are described below, including: Section 1.1 for Development and Land Use Patterns’ and ‘Settlement Areas’; Section 1.3 for ‘Employment’; Section 1.7.1 for ‘Long- Term Economic Prosperity’; Section 1.8 for ‘Energy Conservation, Air Quality and Climate Change’; and Section 2.1 for ‘Natural Heritage’.

The proposed development is consistent with the Provincial Policy Statement for several compelling reasons:

- Efficient land use is promoted in designated Settlement Areas to encourage new growth and development.
- The development includes a variety of employment uses to enhance economic sustainability, contributing to the objectives of the Town of Milton and the Region of Halton in diversifying land use.
- The project's design emphasizes the efficient utilization of both existing and planned infrastructure, enhancing resource sustainability.
- By incorporating three warehouse buildings with associated offices and supporting commercial uses, the development aligns with the economic and long-term requirements of the community.
- The proposal is strategically coordinated with planned developments to the south and west, ensuring the delivery of adequate infrastructure and services, further supporting the community's well-being.
- The location of the development is supportive of public transportation, reducing dependence on personal vehicles.
- The proposed development shows commitment to preserving the ecological integrity of the area by realigning channel blocks to recognize and safeguard the NHS.

#### **4.3 Proposed Provincial Planning Statement (2023)**

On April 6, 2023, the Government of Ontario made public the draft version of the Provincial Planning Statement 2023, which is identified as PPS 2023. This marks the beginning of a review period during which the public can submit their comments. The main aim of the PPS 2023 is to combine and replace the existing Provincial Policy Statement 2020, also known as PPS 2020, and "A Place to Grow: Growth Plan for the Greater Horseshoe," that was last updated in 2020. The PPS is a crucial policy framework that governs land use planning and development throughout the province. It establishes the essential principles and objectives that support sustainable growth, resource conservation, and various land use considerations in Ontario.

Although the PPS 2023 is not yet finalized or officially adopted, it is a crucial point of reference for the proposed development currently under consideration. This is because the draft PPS 2023 provides early insights into the provincial government's evolving approach to growth policies, especially those related to employment and employment areas. These policies have a significant impact on the development landscape, shaping land use, infrastructure development, and economic expansion.

A noteworthy observation is that the proposed PPS 2023 introduces modifications and renumbering of policies related to employment and employment areas, in contrast to the PPS 2020. This underscores the

importance of diligent scrutiny by policymakers and developers to ensure that their plans and initiatives align with the evolving provincial vision for growth.

In addition, the PPS 2023 includes new sections that are important for the proposed development. These sections are expected to provide clear guidelines and requirements that the development must follow. This will improve transparency and accountability in the planning and approval processes.

While the PPS 2023 remains in the review stage, it fulfills a pivotal role as a reference point for comprehending the ever-evolving landscape of land use and development policies within Ontario. Policymakers, developers, and stakeholders are closely monitoring the finalization of the PPS 2023, recognizing its pivotal role in shaping the management of growth, particularly in the realm of employment-related developments, throughout the province. The forthcoming PPS 2023 promises to significantly influence the trajectory of growth and development across Ontario, underscoring the need for careful consideration and adaptation by all involved parties.

In the PPS 2023, additional sections have been added and which may be relevant to the proposed development:

## 2.8 Employment

### 2.8.1 Supporting a Modern Economy:

#### *1. Planning authorities shall promote economic development and competitiveness by:*

- d) encouraging intensification of employment uses and compact, mixed-use development that incorporates compatible employment uses such as office, retail, industrial, manufacturing and warehousing, to support the achievement of complete communities.*

### 2.8.2 Employment Areas

#### *1. Planning authorities shall plan for, protect and preserve employment areas:*

- b) That are located in proximity to major goods movement facilities and corridors, including facilities and corridors identified in provincial transportation plans, for the employment area uses that require those locations.*

#### *2. Planning authorities shall designate, protect and plan for all employment areas in settlement areas by:*

- a) Planning for employment area uses over the long-term that require those locations including manufacturing, research and development in connection with manufacturing, warehousing and goods movement, and associated retail and office uses and ancillary facilities;*

The proposed development's alignment with the forthcoming PPS 2023 highlights its commitment to supporting employment and economic growth in a manner that is consistent with the provincial policy direction. The development's focus on warehousing facilities, associated offices, and complimenting commercial/retail, strategically located near crucial transportation infrastructure, mirrors the objectives of creating vibrant Employment Areas as envisioned in the draft policy. By doing so, it not only prepares for the future regulatory framework but also contributes to the broader vision of fostering sustainable and efficient employment-centric land use planning. This alignment assures that the proposed development is

well-positioned to adhere to the policies and principles of the upcoming PPS 2023, thereby contributing to the responsible and forward-thinking approach to land use and economic development.

#### **4.4 Growth Plan of the Greater Golden Horseshoe (2020)**

The Growth Plan for the Greater Golden Horseshoe (2019), as consolidated in the Office Consolidation (2020), officially took effect on August 28, 2020. This plan is a blueprint for achieving Ontario's vision of creating more successful and resilient communities by managing growth in the Greater Golden Horseshoe Region until 2041. The plan aims to establish complete communities that are compact, accessible by transit, and use infrastructure and public service facilities efficiently. Additionally, it seeks to increase housing availability, promote economic growth, simplify approval processes, and protect natural heritage features and agricultural lands.

The location of the subject site is particularly important, as it falls within the 'Settlement Area' and has been designated as a 'Designated Greenfield Area,' as shown in Schedule 2 of the Place to Grow Concept. Additionally, it is situated within Provincially Significant Employment Zone 18. The proposed development is in line with the Growth Plan's vision. Below are the relevant policies.

##### *“1.2.1 Guiding Principles*

- *Support the achievement of complete communities that are designed to support healthy and active living and meet people’s needs for daily living throughout an entire lifetime;*
- *Provide flexibility to capitalize on new economic and employment opportunities as they emerge, while providing certainty for traditional industries, including resource-based sectors;*
- *Improve the integration of land use planning with planning and investment in infrastructure and public service facilities, including integrated service delivery through community hubs, by all levels of government.*
- *Protect and enhance natural heritage, hydrologic, and landform systems, features, and functions.”*

The proposed development aligns with the Guiding Principles of the GGH for several compelling reasons. This proposal plays a pivotal role in supporting the long-term economic sustainability of the community. It achieves this by creating employment opportunities for both current and future residents of the Town of Milton, thereby fostering economic growth and stability within the region.

In addition, the development demonstrates a commendable commitment to collaboration and coordination with neighbouring landowners. This cooperative approach ensures the seamless delivery of essential services and amenities to the community, enhancing its overall livability and functionality.

Furthermore, the proposal highlights its dedication to the preservation of natural assets by planning to channel the tributaries of Sixteen Mile Creek through the property. This environmentally conscious measure not only safeguards the NHS but also optimizes the layout of development blocks for greater efficiency.

It is considered that the development proposal actively promotes complete communities, fuels economic success, engages in effective coordination with neighbouring stakeholders, and embraces sustainable practices to protect natural assets—all of which contribute to the holistic improvement of the community and the broader region.

*“2.2.1 Managing Growth*

- 2. Forecasted growth to the horizon of this Plan will be allocated based on the following:*
  - a) the vast majority of growth will be directed to settlement areas that:*
    - ii. have existing or planned municipal water and wastewater systems; and*
    - iii. can support the achievement of complete communities.*
  - c) within settlement areas, growth will be focused in:*
    - ii. strategic growth areas;*
    - iii. locations with existing or planned transit, with a priority on higher order transit where it exists or is planned; and*
    - iv. areas with existing or planned public service facilities.*
- 4. Applying the policies of this Plan will support the achievement of complete communities that:*
  - a) feature a diverse mix of land uses, including residential and employment uses, and convenient access to local stores, services, and public service facilities;*
  - b) improve social equity and overall quality of life, including human health, for people of all ages, abilities, and incomes*
  - a) expand convenient access to:*
    - ii. public service facilities, co-located and integrated in community hubs;*
    - iii. an appropriate supply of safe, publicly-accessible open spaces, parks, trails, and other recreational facilities;”*

The proposed development aims to strategically channel growth towards a designated 'Settlement Area.' This area is well-planned and equipped with essential public service facilities, a transit system, and reliable municipal water and wastewater infrastructure. This ensures that new development is concentrated in an area that can handle increased demands.

One of the significant strengths of this development is its carefully designed transportation network. It provides enhanced and convenient access to various services, facilities, and neighbouring properties. This improves overall accessibility within the community, making it well-connected and easily navigable. The transportation system is a vital component of the development's plan, ensuring that residents and businesses have seamless access to the amenities and resources they need, which, in turn, contributes to the overall functionality and attractiveness of the community.

*“2.2.5 Employment*

- 1. Economic development and competitiveness in the GGH will be promoted by:*
  - a. making more efficient use of existing employment areas and vacant and underutilized employment lands and increasing employment densities;*
  - b. ensuring the availability of sufficient land, in appropriate locations, for a variety of employment to accommodate forecasted employment growth to the horizon of this Plan;*
  - c. planning to better connect areas with high employment densities to transit; and*
  - d. integrating and aligning land use planning and economic development goals and strategies to retain and attract investment and employment.*
- 2. Major office and appropriate major institutional development will be directed to urban growth centres, major transit station areas or other strategic growth areas with existing or planned frequent transit service.*

3. *Retail and office uses will be directed to locations that support active transportation and have existing or planned transit.*
7. *Municipalities will plan for all employment areas within settlement areas by:*
  - a. *prohibiting major retail uses or establishing a size or scale threshold for any major retail uses that are permitted and prohibiting any major retail uses that would exceed that threshold; and*
  - b. *providing an appropriate interface between employment areas and adjacent nonemployment areas to maintain land use compatibility.*
1. *The Minister may identify provincially significant employment zones and may provide specific direction for planning in those areas to be implemented through appropriate official plan policies and designations and economic development strategies.”*

The proposed development is strategically located within Provincially Significant Employment Zone 18. This area has been identified and safeguarded by the Province, Region, and Town for employment growth to promote economic development and job creation.

The proposed Draft Plan of Subdivision aims to subdivide currently vacant greenfield land adjacent to Derry Road, which is a major arterial regional road. The subdivision plan will facilitate the creation of three business park blocks, one commercial/retail block, one SWM pond block with two SWM detention basins, and one public road. This approach ensures the availability of adequately sized land parcels in an appropriate location, creating an environment that's conducive to employment growth.

The proposed development shows a high degree of coordination with adjacent properties within the employment area. This collaborative effort contributes to the efficient development of the employment area and ensures a seamless and harmonious interface with non-employment lands. This holistic and cooperative approach further sets the development's role in promoting sustainable and balanced growth in the region.

#### *“2.2.7 Designated Greenfield Areas*

1. *New development taking place in designated greenfield areas will be planned, designated, zoned and designed in a manner that:*
  - a. *supports the achievement of complete communities;*
  - b. *supports active transportation; and*
  - c. *encourages the integration and sustained viability of transit services.*
2. *The minimum density target applicable to the designated greenfield area of each upper-and single-tier municipality is as follows:*
  - a) *The Cities of Barrie, Brantford, Guelph, Hamilton, Orillia and Peterborough and the Regions of Durham, Halton, Niagara, Peel, Waterloo and York will plan to achieve within the horizon of this Plan a minimum density target that is not less than 50 residents and jobs combined per hectare;”*

The proposed Draft Plan of Subdivision is located within the designated Greenfield Area of the Town of Milton. This development is set to expand the range of land uses within the town and contribute to the broader vision of creating a well-rounded and comprehensive community.

In collaboration with neighbouring landowners, the proposal includes an extension of the existing Clark Boulevard up to Derry Road. This extension serves to enhance the overall connectivity of the transportation

network, making it easier for residents, workers, and businesses to navigate the area. By improving transportation infrastructure, the development contributes to a more accessible and functional community.

Furthermore, the proposed development aligns with the Town and Region's aspirations to achieve a density of 50 residents and jobs per hectare, as stipulated by the Growth Plan. This targeted density level is an essential component of smart growth planning, and the development's inclusion in this endeavor underscores its commitment to promoting sustainable and well-balanced development in the region.

In summary, the proposed industrial Draft Plan of Subdivision not only complements the existing Greenfield Area but also fosters greater connectivity and accessibility. Additionally, it contributes to the broader goals of achieving optimal density in line with regional growth objectives.

#### *“4.2.2 Natural Heritage System*

##### *3. Within the Natural Heritage System:*

- a. New development or site alteration will demonstrate that*
  - ii. There are no negative impacts on key natural/heritage features or key hydrogeologic features or their functions;*
  - iii. Connectivity along the system and between key natural heritage features and key hydrologic features located within 240 metres of each other will be maintained or, where possible, enhanced for the movement of native plants and animals across the landscape;*
  - iv. the removal of other natural features not identified as key natural heritage features and key hydrologic features is avoided, where possible. Such features should be incorporated into the planning and design of the proposed use wherever possible;”*

The proposed development aims to channel the tributaries of the Sixteen Mile Creek for two main reasons: to preserve and connect the NHS and to optimize the layout of development blocks for greater efficiency.

The channel blocks, which are around 60-65 meters (167-213 feet) wide and surrounded by a 10-meter (33 feet) buffer zone, have been designed meticulously to ensure that they do not have any adverse impacts on the NHS. Detailed Site Impact Studies (SIS) and Functional Servicing Reports (FSR) will provide further information on the appropriate grading, setbacks, and landscaping measures necessary to safeguard and enhance the transition areas around the NHS.

Importantly, the proposed development aligns seamlessly with the established policies of the Growth Plan for the Greater Golden Horseshoe for several compelling reasons:

- **Strategic Location:** The development is strategically situated within a 'Settlement Area' and a 'Designated Greenfield Area.' These designations offer an appropriate and well-placed setting to support employment uses.
- **Provision for Job Growth:** The proposed Draft Plan of Subdivision creates three business park blocks within Provincially Significant Employment Zone 18, thereby facilitating job growth while ensuring the continued protection of lands designated for employment purposes.
- **Density Target Achievement:** The development aids in achieving the minimum density target outlined by the Region of Halton, promoting efficient land use and urban density.

- **Efficient Infrastructure Use:** The design of the development is optimized to efficiently utilize existing and planned infrastructure, minimizing resource consumption and servicing costs.
- **Economic Development Support:** By encouraging employment uses, the proposal actively contributes to the fostering of long-term economic development in the region.
- **NHS Preservation:** Recognizing and safeguarding Natural Heritage Systems is a pivotal aspect of the proposal, underscoring its commitment to creating healthy and active communities.

The proposed development is consistent with the Growth Plan's policies through its strategic location, commitment to job growth, density target achievement, efficient infrastructure use, support for economic development, and the preservation of natural heritage systems. These attributes collectively reflect the development's dedication to responsible and sustainable growth within the Greater Golden Horseshoe region.

#### **4.5 Region of Halton Official Plan**

The Region of Halton Official Plan (ROP) is a crucial document that shapes the physical development of Halton Region. It serves as a guide to address both the immediate and long-term needs of its residents by offering a comprehensive vision for the region's physical layout and community character. The plan includes a set of objectives and policies aimed at establishing an urban structure capable of accommodating future growth effectively. It is worth mentioning that ROP Amendment 49 (ROPA 49), which was modified by the Province, was officially adopted on November 2, 2022. This amendment has significantly influenced the policy framework within which our evaluation takes place.

As illustrated in **Figure 4 - Map 1 Regional Structure**, the subject lands are designated as both 'Urban Area' and 'Regional Natural Heritage System (RNHS)'. **Figure 5 - Map 1H Regional Urban Structure** provides further detail of the Urban Area's specifics, designating these lands as 'Employment Area'. Notably, these lands are strategically situated to the south of a major commuter and commercial rail corridor, home to a Canadian Pacific (CP) intermodal yard and an upcoming Major Transit Station Area (MTSA). Additionally, they are positioned to the west of a Regional Intensification Corridor within the Regional Urban Boundary.

The significance of this positioning can be understood in several ways. Firstly, it demonstrates a forward-thinking approach to land use planning in the region by ensuring that areas designated for employment align with critical transportation infrastructure, leading to enhanced accessibility and connectivity. Secondly, the proximity to a Major Transit Station Area highlights the commitment to promoting sustainable and efficient transportation options through transit-oriented development. Lastly, the designation of these lands within a Regional Intensification Corridor emphasizes the region's focus on compact and intensified development, which is a crucial element in modern urban planning to optimize land use and resource efficiency.

The ROP serves as a foundational document guiding the physical growth and development of Halton Region. The lands' designation as 'Urban Area' and 'Employment Area' and their strategic location in proximity to vital transportation nodes demonstrate a commitment to forward-looking and sustainable urban development principles. This approach aligns with contemporary planning ideals focused on efficient land use, enhanced transportation options, and community livability.

Our proposed development seamlessly aligns with the regional policies outlined in the Region of Halton Official Plan, supported by compelling reasons:

- Our proposal aims to promote the growth and development of employment lands within the 'Urban Area'. This is a strategically located zone that is well-equipped with urban services to facilitate future expansion. This alignment ensures that these lands receive the necessary urban infrastructure required to support their growth and development effectively.
- The proposal has the potential to make a significant contribution to our goal of promoting diverse and complete communities. By introducing employment lands, it can provide an economic boost that can invigorate the local economy and contribute to the overall vitality of the region.
- Our proposal not only aligns with the region's ecological goals but actively contributes to the safeguarding and enhancement of the NHS. This is achieved by creating delineated NHS blocks and realigning channel blocks, which efficiently shapes the employment blocks to support the region's environmental objectives.
- The estimated creation of approximately 1,700 jobs, based on employment density calculations from Halton Region's Investment Readiness and Employment Study, signifies a substantial contribution to the employment targets established for the Town of Milton Designated Greenfield Area.
- The site's location near a Regional Intensification Corridor within the Regional Urban Boundary and its proximity to major 400 series highways facilitate efficient transportation of goods in the employment area, promoting economic efficiency and regional connectivity.
- In terms of accessibility, the proposed development enjoys a prime location in close proximity to existing and planned residential neighborhoods, including a future MTSA. It is also well-connected by major roadways, ensuring seamless access for both the workforce and the efficient movement of goods and services.

The proposed development conforms with the regional policies set out in the Region of Halton Official Plan. It not only promises to boost the economic growth of the region, but it also strategically integrates with the existing and future urban framework, ensuring a cohesive and sustainable development that caters to the needs of both current and future residents.

#### **4.6 Town of Milton Official Plan**

On June 14, 2014, the Milton Town Council officially adopted Official Plan Amendment No. 31 (OPA 31). This was an important step in aligning the Town's Official Plan with the Growth Plan for the Greater Golden Horseshoe and the Sustainable Halton Plan. The Region of Halton later made some adjustments to the policies outlined in OPA 31. On June 18, 2018, Milton Town Council endorsed these modifications proposed by the Region of Halton. This led to the incorporation of these changes into the official planning framework.

It is worth noting that on November 9, 2015, the Town of Milton passed Bylaw 088-2015 to establish the C.9 Derry Green Corporate Business Park Secondary Plan (DGCBP). As a result, the policies articulated in OPA 31 and the Secondary Plan DGCBP are now in full legal effect.

The land in question is categorized into two designations, as illustrated in **Figure 8 - Schedule C-9-B: DGCBP Land Use Plan**. These designations are 'Business Park Area' and 'Natural Heritage System.'

Furthermore, Schedule I1 - Urban Area and Specific Policy Areas provides additional detail, designating the northeastern section of the subject lands as part of 'Special Policy Area 10' as illustrated in **Figure 6** and **Figure 9** respectively. Within this specific policy area, building heights are restricted to a maximum of 18 metres (59 feet) within a 4-kilometre radius of the AM Radio transmission facilities. This regulatory framework defines the permitted land uses and development standards for the subject lands, ensuring that they adhere to the established planning guidelines and considerations.

Relevant policies applicable to this application in the Official Plan includes the following;

### **2.1.3 Municipal Structure and Community Context**

2.1.3.2 *“Urban Area [...] Significant employment areas accommodating a range of office, manufacturing, logistics and other employment uses are located along the Highway 401 corridor and the east side of the Urban Area. Major employment opportunities will continue to be fostered in these locations, and expansions to the employment land base will be considered to maintain a sustainable balance between employment and residential growth.”*

### **2.1.4 Growth Management**

2.1.4.1.1 *“In accordance with the policies of the Growth Plan for the Greater Golden Horseshoe, the policies of this Plan are intended to plan and manage growth and support the development of a compact, vibrant and complete urban community while protecting and preserving the surrounding agricultural and natural heritage landscapes.”*

2.1.4.1.2 *This Plan contains policies to guide land use planning and development decisions through to the 2031 horizon year based upon the following population and employment targets.”*

The envisioned project is situated on the eastern side of the Urban Area, strategically positioned to capitalize on its convenient access to both Highway 401 and 407. This advantageous location forms a key element of the development's appeal. The proposed project, as outlined in the conceptual site plan, is slated to introduce an expansive 182 000 square metres (449 732 square feet) of industrial/warehouse space and 9 000 square metres (22 240 square feet) office-associated office space to the area.

The proposed development has the potential to create approximately 1,700 jobs using the ratio provided in the Halton Region's Investment Readiness and Employment Study from December 23, 2020. This aligns with the town's employment targets and contributes significantly to local economic objectives.

The project is economically beneficial and also contributes to a dynamic and comprehensive community by integrating employment-focused infrastructure into the Designated Greenfield Area. Such a multifaceted contribution aligns with broader urban planning goals, promoting a community that is not only economically vibrant but also socially and environmentally sustainable.

The development's merit lies in its ability to boost the community while also preserving vital NHS lands. The accompanying SIS provides details about the protective measures taken. Therefore, the project not only adds an employment dimension to the designated area but also operates in harmony with broader community development objectives by preserving and respecting existing NHS lands. This dual focus on economic advancement and environmental responsibility highlights the comprehensive and thoughtful nature of the proposed development.

## **2.8 Urban Design**

### **2.8.2 Objectives**

- “2.8.2.1 To practice sustainable urban development by adhering to sustainable design principles and standards which respect the natural bioregion, reinforce natural processes and conserve natural resources.*
- 2.8.2.2 To achieve a consistently high standard of design in the built environment that is complementary to and compatible with existing development and the Town’s natural and cultural heritage in all areas including site, building and landscape design.*
- 2.8.2.3 To develop an active and attractive network of urban spaces by ensuring mutually supportive relationships between open spaces and the built forms which enclose them.”*

The development plan is focused on creating an attractive streetscape and promoting a mutually beneficial relationship between the constructed structures, open spaces, and the adjacent NHS surroundings. These design principles are described in detail in the Urban Design Brief.

The commitment to an appealing streetscape involves a considerate approach to the visual and aesthetic elements of the development that will interact with the public. This may include architectural design, landscaping, street furniture, and overall urban aesthetics. The goal is to positively contribute to the visual character of the surrounding area, creating a lively and engaging atmosphere for residents and visitors alike.

The Urban Design Brief is a guiding document that outlines how the development will adhere to these design principles. It includes specific details on architectural styles, landscaping plans, the arrangement of open spaces, and measures to ensure the development complements and respects the adjacent NHS area. This comprehensive approach to urban design not only enhances the immediate visual appeal of the development, but also aligns with broader goals of sustainable and harmonious urban living.

## **3.8 Business Park Area**

### **3.8.1 GENERAL \*DI(viii)**

#### *PURPOSE*

- “3.8.1.1 The Business Park Area designation on Schedule “B” is an employment designation which applies to areas where the full range of light industrial and office uses will be permitted, subject to a high standard of design.”*

### **3.8.2 PERMITTED USES \*DI(viii)**

- 3.8.2.1 The Business Park Area designation on Schedule “B” means that the main permitted uses shall be light industrial and office uses.*
- 3.8.2.2 In addition to the main permitted uses within the Business Park Area designation, the following uses shall also be permitted:*
- a) Accessory service, wholesale, retail and office uses directly related to the industrial use and located within the industrial building;*

- b) Research and development uses excluding uses which produce biomedical waste; and,*
- c) Restaurants that are part of and are located wholly within a light industrial or office building, other than an industrial mall.*

**3.8.3 BUSINESS PARK AREA POLICIES \*D1(viii)**

*3.8.3.1 Development within areas designated “Business Park Area” within the Established Urban Area on Schedule “B” or within an approved Secondary Plan, shall be permitted subject to:*

- a) the provisions of the applicable Secondary Plan as outlined within Part C of this Plan;*
- b) the submission of a development plan which demonstrates that the proposed development can be physically integrated with existing and proposed uses of adjacent lands, including lands outside the Business Park Area designation;*
- c) no outdoor storage being allowed;*
- d) a high quality of landscaped site development particularly adjacent to Provincial Freeways, Highways, Major Arterial, Minor Arterial or Multi-Purpose Arterial roads;*
- e) the proposed development complies with the Community-wide policies of Section 2.0 of this Plan; and,*
- f) applicants can demonstrate that there is adequate wastewater and water treatment capacity to accommodate the proposed use.*

The proposed development, situated within the Derry Green Corporate Business Park Secondary Plan area, includes warehouses, accompanying office spaces, and complementing commercial/retail use, aligning with the designated land use regulations specified for the 'Business Park Area.' This adherence to zoning regulations ensures that the intended land use is in harmony with the defined purpose of the area as outlined in the Derry Green Corporate Business Park Secondary Plan.

A key aspect emphasized in the accompanying Urban Design Brief is the commitment to designing the development in a manner that is compatible with adjacent properties. This commitment signifies a conscientious effort to integrate the proposed structures seamlessly into the existing urban landscape, fostering a cohesive and visually harmonious relationship with neighbouring properties. The promise of a high-quality landscaped site design further indicates a dedication to aesthetic considerations and greenery, enhancing the overall visual appeal of the development and its surroundings.

The preparation of a Functional Servicing Report (FSR) adds a layer of assurance to the project's viability. The FSR is instrumental in demonstrating that the necessary infrastructure, particularly for wastewater and water treatment, is not only present but also possesses the capacity to accommodate the demands of the proposed development. This ensures that the development aligns with the infrastructure capacity stipulated in the Derry Green Corporate Business Park Secondary Plan, preventing undue strain on local services.

The proposed development's conformity with zoning regulations, commitment to compatibility with adjacent properties, adherence to high-quality landscaped design, and support from a Functional Servicing Report collectively underscore a responsible and well-planned urban development approach per the Derry Green Corporate Business Park Secondary Plan.

#### **4.8 Natural Heritage System**

*4.8.1.6 The objectives of the Natural Heritage System are:*

- a) To protect, maintain and enhance natural heritage, hydrologic and landform features and functions including protection of habitat for flora and fauna and particularly species at risk;*
- b) To improve or restore the quality and quantity of ground and surface water and the hydrologic functions of watersheds;*
- c) To protect or enhance Key Features, without limiting the ability of existing agricultural uses to continue;*
- d) To direct development to locations outside of hazard lands;*
- e) To contribute to a continuous natural open space system to provide continuous corridors and inter-connections between the Key Features and their ecological functions and a visual separation of communities;*
- g) To protect significant scenic and heritage resources and preserve the aesthetic character of natural features.*

The proposed development aims to preserve and enhance the connectivity and functionality of the adjacent NHS lands. To achieve this, two channel blocks will be created, which will seamlessly incorporate landscaped elements, fostering a green corridor that connects to adjacent properties. These channel blocks will not only serve as connectors but will also contribute to the aesthetic enhancement of the surroundings, maintaining a sense of continuity with the natural landscape.

Moreover, the management of urban runoff, a critical aspect of sustainable development, will be addressed through a strategic approach. The proposal outlines the utilization of one SWM pond and two SWM detention basins. These elements collectively aim to manage and control urban runoff effectively. By employing these stormwater management features, the proposed development seeks to mitigate the impact of urban runoff on the NHS lands.

The incorporation of the SWM pond and detention basins is designed to reduce the quantity of urban runoff entering the NHS. This approach aligns with responsible environmental practices and demonstrates a commitment to minimizing potential adverse effects on the natural landscape. The emphasis on stormwater management reflects a holistic approach to development, acknowledging the importance of sustainable water practices to preserve the ecological integrity of the NHS lands.

The proposed development aims to coexist with the adjacent NHS lands and actively contribute to their preservation and enhancement. Through the creation of landscaped channel blocks and the implementation of SWM features, the development seeks to minimize its environmental footprint, ensuring that urban runoff is managed effectively and does not compromise the ecological health of the NHS. This approach

underscores a conscientious effort to integrate sustainable and environmentally friendly practices into the fabric of the proposed project.

**Specific Policy Area No. 10**

*4.11.3.10 The area identified as Specific Policy Area No. 10 on Schedule “11” of this Plan, includes lands within a 4 Kilometre radius of the AM Radio transmission facilities located in Milton near the corner of Trafalgar Road and Highway #401, shall be developed in accordance with the policies of subsections 3.8.3.2 ad 3.9.3.2 of this Plan*

*3.8.3.2 Where new employment uses are proposed within the radius, consideration shall be given by the Town, in consultation with the Canadian Broadcasting Company, to limiting the height of the proposed building(s) to 18 metres or less and to limiting the extent of steel used in the construction of the building(s), to prevent interference of the radio transmission.*

*3.9.3.2 Where new Business Park or Industrial development is proposed within a 4 kilometre radius of the AM Radio transmission facilities located in Milton near the corner of Trafalgar Road and Highway #401, consideration shall be given by the Town, in consultation with the Canadian Broadcasting Company, to limiting the height of the proposed building(s) to 18 metres or less and to limiting the extent of steel used in the construction of the building(s), to prevent interference of the radio transmission.*

The proposed development, as illustrated in the conceptual site plan in **Figure 3**, demonstrates a commitment to adhering to specific height regulations. The warehouse and industrial buildings are designed not to exceed 18 metres (59 feet) in height, aligning with the stipulations of Special Policy Area No.10. Moreover, the Secondary Plan imposes additional height restrictions, and the proposed development is in full compliance with these additional guidelines.

The alignment of the proposed development with the Town of Milton Official Plan is emphasized by several key factors:

**Contribution to Employment Targets:** The proposal actively contributes to the Town's prescribed employment targets for the year 2031. By introducing a mix of warehouse buildings and accessory office uses, the development creates an environment conducive to economic growth and job creation within the community.

**Conformance with Business Park Area Designation Policies:** The proposed development aligns with the policies outlined in the Business Park Area Designation. Specifically, the plan involves the creation of three business blocks to accommodate the construction of three warehouse buildings and accessory office uses. This adherence ensures that the development is in line with the designated purpose of the area.

**Job Creation and Economic Development:** The proposal is designed to facilitate job creation, thereby supporting broader economic development objectives within the Town of Milton. This commitment to generating employment opportunities contributes to the overall vitality and sustainability of the local economy.

**Preservation and Enhancement of NHS Lands:** The development plan incorporates the creation of two channel blocks, as determined by the supporting Environmental Impact Study (EIS). These channel blocks are strategically designed to preserve and enhance the connectivity and functionality of Natural Heritage System (NHS) lands, showcasing a proactive approach to environmental conservation.

**Thoughtful Urban Design:** The proposed development emphasizes thoughtful urban design principles, ensuring that the project not only conforms to existing land uses but also complements both current and future developments. This approach aims to create a cohesive and aesthetically pleasing urban environment.

**Optimization of Infrastructure:** The development plan demonstrates a commitment to optimizing the use of both new and existing infrastructure. This strategic utilization ensures that the proposed project integrates seamlessly with the existing urban framework while minimizing the strain on public services.

In summary, the proposed development conforms to the various elements of the Town of Milton Official Plan, reflecting a comprehensive and well-thought-out approach that not only meets regulatory requirements but also contributes positively to the economic, environmental, and urban development goals of the community.

#### **4.7 Derry Green Corporate Business Park Secondary Plan**

The DGCBP Secondary Plan, initially adopted on June 14th, 2010, by the Town Council and subsequently approved by the Region of Halton with modifications in March 2014, underwent further review and appeals. In November 2015, the plan received final approval from the Ontario Municipal Board (OMB), solidifying its status as the guiding framework for development in the designated area.

The primary goal of the DGCBP Secondary Plan is to facilitate a diverse range of employment uses that contribute to the long-term economic sustainability of the community. This objective is achieved by ensuring the implementation of high-quality development practices across various environments within the designated area. The Secondary Plan provides a robust set of policies and a detailed planning framework that complements and extends the general policy framework outlined in the Town of Milton's Official Plan.

The subject lands fall within the DGCBP Secondary Plan and are designated under three distinct categories: 'Business Park Area,' 'Environmental Linkage,' and 'Natural Heritage System' as illustrated in **Figure 8** – Schedule C-9-B DGCBP Land Use Plan. Specific overlays further define development considerations for different portions of the subject lands. For instance, lands designated as 'Business Park Area' at the intersection of Derry Road and Sixth Line are subject to the 'Gateway' overlay, while other lands along Derry Road have the 'Street Orientated Site' overlay. The lands south of the Natural Heritage System, situated north of the Union Gas Pipeline, are subject to the 'Natural Heritage Oriented Area' overlay. Additionally, potential locations for a Stormwater Management (SWM) pond are identified within the southern half of the property.

The subject lands are located in Phase 2 of the DGCBP Phasing Plan, as shown in **Figure 10**. This plan outlines the order and timing of development activities, ensuring a coordinated and systematic implementation of the DGCBP Secondary Plan.

The DGCBP Secondary Plan serves as a comprehensive and thoroughly designed framework to guide the development of the subject lands. It not only defines land use designations but also incorporates overlays and phasing plans to address specific considerations for different areas within the designated zone, emphasizing a strategic and integrated approach to community development.

Relevant Policies applicable to the proposed development are highlighted and summarized herein.

## **C.9.2 PLANNING DISTRICT CONCEPT**

### **C.9.2.1 CORPORATE BUSINESS PARK CHARACTER**

*C.9.2.1.1 The Secondary Plan is designed to create a comprehensively planned, high quality, corporate business park with a significant component of office and other prestige uses. It will have approximately 17-18,000 employees based on an assumption of approximately 45 employees per net hectare. This area will also serve as a gateway to the Urban Area and, as such, will reflect the strong sense of community and the environment evident in Milton today by:*

- a) providing a multi-modal transportation system, including road, rail, transit and trail/ path facilities, and a development pattern which supports that system, to ensure the maximum degree of physical connection to key market areas, as well with the Existing Milton Urban Area, within the Planning District, and with other surrounding areas of the Town;*
- b) ensuring that development fronting on major roads, and the road allowances, achieves high urban design standards to reinforce the Town's image and provide an attractive entrance to the community;*
- c) providing the flexibility to accommodate a broad range of potential uses in the Corporate Business Park as a whole, while at the same time, establishing appropriate controls on development to ensure that high quality and prestige uses locate at key locations such the areas fronting on Highway 401, Derry Road and James Snow Parkway, and to minimize potential conflicts between uses, including conflicts which relate to the character of specific areas of the Corporate Business Park and the relationship to residential development on the west side of James Snow Parkway;*
- d) creating a Natural Heritage System within the Planning District which is connected to the Natural Heritage System and parkland in other areas of the Town, particularly the Provincial Greenbelt to the east and south of the Planning District.*

The proposed development aims to provide high-quality employment lands in the designated area. The transportation network has been strategically designed to maximize efficiency. This is planned through the establishment of convenient access points via Derry Road and Sixth Line, as well as the extension of Clark Boulevard. The integration of these access points is expected to improve the overall connectivity of the development, making transportation flow smoother within and around the site.

Based on calculations from the Investment Readiness and Employment Study by Halton Region, dated December 23, 2020, it is estimated that the proposed development will create around 1,700 job opportunities. This aligns with the employment targets stated in the DGCBP Secondary Plan, and by contributing to job creation, the development will support the long-term economic objectives outlined in the plan.

The proposed development will have a positive impact on the economy, but it also prioritizes urban design. The design is not only functional but also aesthetically pleasing, with particular attention given to the facade facing the street. The goal is to create a built environment that is not only functional but also blends in seamlessly with the surrounding urban area. This will contribute positively to the overall urban fabric and align with current, and future uses nearby.

One important aspect of the development plan is the realignment of channel blocks, which has a dual purpose. Firstly, it connects to adjacent properties. Secondly, it improves the connectivity and functionality of the NHS within the DGCBP Secondary Plan. This approach demonstrates a commitment to environmental stewardship and sustainable development by promoting the preservation and enhancement of natural features in the area.

The proposed development project aims to benefit the community in multiple ways. It will offer employment opportunities, support transportation efficiency goals, and prioritize visually appealing and contextually appropriate urban design. Additionally, the channel blocks' realignment highlights the project's dedication to improving the natural environment, promoting connectivity, and achieving the overall goals outlined in the DGCBP Secondary Plan.

### **C.9.2.2 KEY DESIGN ELEMENTS**

*C.9.2.2.1 The following key design elements form the basis of the Secondary Plan as outlined on Schedule "C.9.A", Derry Green Corporate Business Park Structure Plan. They include:*

- a) Natural Heritage System A Natural Heritage System consisting of habitat complexes, watercourse corridors and buffers is a central feature of the community protecting key environmental features and where feasible providing a connection to the Natural Heritage System in the rest of the Urban Area, as well as to the surrounding rural area and, in particular, the Provincial Greenbelt to the east and south of the Secondary Plan Area. The road system will be aligned to give appropriate accessibility to the Natural Heritage System both physically and visually.*
- c) Connectivity/Accessibility The Secondary Plan Area includes numerous barriers to connectivity and accessibility, both externally and internally, (e.g. Highway 401, gas line and the railway). Improved internal and external access for vehicles, but also for pedestrians and cyclists, strengthens the area's integration with the rest of the community, as well as enhancing its gateway function and marketability. The road system within the framework of the transportation system designated on Schedules "C.9.A" and "C.9.B" will be designed with a modified grid pattern to enhance connectivity and accessibility. Key planned improvements to the transportation system designed to overcome barriers to connectivity and accessibility include:
  - i) a grade separation at the railway for Fifth Line;*
  - ii) pedestrian/bicycle bridge over the Middle Branch of the Sixteen Mile Creek at the gas pipeline;**

- iii) *road connections to James Snow Parkway opposite existing roads on the west side of the Parkway;*
- e) *Enhanced Streetscape Design James Snow Parkway and Derry Road represent major access routes into and through the community. In addition, each of these roads has a significant role in the community:*
  - ii) *Derry Road is also a major gateway to the Urban Area from the east. As such, it is important in creating the image of the community, that these streets have an enhanced level of streetscape design including coordinated street furniture and landscaping. The Town will work with the Region, recognizing that these are Regional roads, to achieve this objective.*
- g) *Gateways*

*The intersections of Main Street and James Snow Parkway; Derry Road and James Snow Parkway; Louis St. Laurent Boulevard and James Snow Parkway; and Derry Road and Sixth Line are recognized as key points of entry or "gateways" to the Town which will require special design treatment both of the road right-of-way and any development adjacent to the right-of-way.*

The proposed development has been designed in collaboration with the approved Subwatershed Impact Study (SIS), ensuring that it is in harmony with the proposed road system and features of the NHS. The SIS serves as a crucial tool to assess and address the impact of the development on the local subwatershed, guiding the design process to ensure environmental compatibility.

The proposed development aims to address the transportation challenges described in the Secondary Plan. Through collaboration with adjacent landowners, the development will facilitate the extension of Clark Boulevard west of James Snow Parkway. This joint effort is meant to improve connectivity within and beyond the Derry Green Corporate Business Park, which will contribute to a more efficient and integrated local transportation network.

The Urban Design Brief, presented alongside the proposal, outlines the commitment to providing an enhanced streetscape design along Derry Road. This includes specific measures such as appropriate screening of parking areas from the street, as well as other design elements, to create an aesthetically pleasing and pedestrian-friendly environment. The attention to streetscape design is aligned with the principles outlined in the SIS, ensuring that environmental considerations are integrated into the overall urban design.

Particular emphasis is placed on the business park block at the intersection of Derry Road and Sixth Line, recognized as a gateway into the DGCBP Secondary Plan. The proposal outlines enhanced design treatments for this area, acknowledging its significance and aiming to create a distinctive and welcoming entry point. Detailed design aspects for business park blocks will be further delineated through the future site plan approval process, ensuring adherence to the DGCBP Secondary Plan Design Guidelines and considering the findings of the Subwatershed Impact Study.

In summary, the proposed development, guided by the Subwatershed Impact Study, highlights a comprehensive approach to design, encompassing environmental impact, transportation considerations, and urban aesthetics. The collaborative efforts with the SIS and coordination with adjacent landowners collectively reflect a commitment to responsible and sustainable development within the DGCBP community.

### **C.9.3 VISION AND OBJECTIVES**

*Further to, and in accordance with, the goals and objectives of Section 2 of this Plan, the following vision and objectives are applicable to the Derry Green Corporate Business Park Planning District.*

#### **C.9.3.1 VISION**

*C.9.3.1.1 The Derry Green Corporate Business Park will be based on strong design requirements to ensure the achievement of quality development, while providing for a full range of employment uses in a variety of different environments. These will include prestige offices, campus settings in close proximity to the Natural Heritage System; street related employment uses and light industrial developments.*

#### **C.9.3.2 OBJECTIVES**

*C.9.3.2.1 To create a Corporate Business Park that capitalizes on Milton's competitive advantage in the Greater Toronto Area market and its location adjacent to the Highway 401 corridor by establishing a development framework that:*

- a) streamlines approvals;*
- b) is flexible in dealing with various types and forms of land uses and buildings while directing prestige uses to key locations; and,*
- c) promotes a program of urban design that is attractive and financially responsible*

*C.9.3.2.4 To create, in consultation with the Region of Halton and Conservation Halton, a linked Natural Heritage System, including a trail system, which is connected with other areas of the Town, forms a central feature of the Corporate Business Park, protects and enhances key existing natural features, and which is easily accessible and visible to residents and visitors.*

*C.9.3.2.10 To work with the Region to design James Snow Parkway and Derry Road, and key intersections on those roads, to reflect their roles as major "gateways" to the Milton Urban Area.*

The proposed development is aimed at diversifying the employment landscape within the DGCBP area by creating warehouse, supporting office and complimenting commercial/retail uses. This addition to the range of employment opportunities aligns with the goals and objectives outlined in the DGCBP Secondary Plan. The diversification of employment uses is a crucial factor in fostering a dynamic and sustainable economic environment within the designated area.

The protection of the NHS is an integral aspect of the proposal. This is achieved through the creation of realigned channel blocks that intricately connect with adjacent properties. The thoughtful design approach not only preserves the NHS but also enhances its overall connectivity. By linking the realigned channel blocks to adjacent properties, the proposal actively contributes to the integration and functionality of key natural features, reinforcing a commitment to environmental conservation.

To enhance the aesthetic appeal and reinforce the development's significance, the proposal includes plans for enhanced landscaping and building facades along the Derry Road frontage. This strategic design approach serves to create an attractive and visually appealing gateway. By investing in landscaping and architectural aesthetics, the proposal aims to underscore the importance of Derry Road as a major gateway into the DGCBP. This not only contributes to the visual character of the area but also reflects a commitment to creating a positive first impression for visitors and residents alike.

The proposed development conforms with the objectives of the DGCBP Secondary Plan by diversifying employment opportunities, protecting the NHS through thoughtful design, and enhancing the visual appeal of the Derry Road frontage. This multifaceted approach reflects a comprehensive understanding of the goals set forth in the Secondary Plan and demonstrates a commitment to responsible and sustainable development within the DGCBP community.

#### **C.9.4.3.1 FUTURE MAJOR ARTERIAL ROAD STUDY AREA**

*The Secondary Plan identifies a north/south local collector road within the area between Fifth and Sixth Lines, extending from Britannia Road to Provincial Highway 401. The need for this collector road will be reviewed in consultation with the Region of Halton prior to its construction. If the Region's Corridor Study determines that a Regional arterial road will be located within this area and that the local collector road is no longer required, the collector road will be deleted without amendment to the Plan.*

The Functional Stormwater and Environmental Management Strategy (FSEMS) has verified that Sixth Line does not require diversion in this specific location, indicating that the proposed development can proceed without altering the course of Sixth Line. This conclusion reflects a thorough assessment of stormwater and environmental management considerations.

In alignment with broader regional planning, the Region's Transportation Master Plan has identified the necessity for an additional north/south 6-lane local road between Fifth and Sixth Line, informally referred to as "5 ½ line." To address this requirement, the proposed Draft Plan of Subdivision, as illustrated in Figure 2, outlines the introduction of a new north/south local collector road named Clark Boulevard. This proposed road aligns with the guidelines established in the DGCBP Secondary Plan, as shown in **Figure 8**. Additionally, the plan includes a north-south extension of Clark Boulevard connecting from Fifth Line.

This strategic road network planning not only adheres to regional transportation priorities but also aligns with the specific provisions outlined in the DGCBP Secondary Plan. The introduction of Clark Boulevard serves to enhance connectivity within the development and, more broadly, contributes to the overall transportation infrastructure outlined in the regional master plan.

The confirmation from the Functional Stormwater and Environmental Management Strategy regarding the unnecessary diversion of Sixth Line, alongside the proposal for a new north/south local collector road

(Clark Boulevard) in compliance with regional plans and the DGCBP Secondary Plan, highlights a comprehensive and coordinated approach to transportation and infrastructure planning within the development area. This alignment ensures that the proposed road network fulfills both local and regional needs while adhering to established planning frameworks.

#### **C.9.4.5 SIXTEEN MILE CREEK SUBWATERSHED UPDATE STUDY**

*C.9.4.5.1 All new development within the Derry Green Corporate Business Park shall be consistent with the recommendations of the SUS, including the use of Low Impact Development Practices. Functional recommendations, derived from the SUS principles and specifically focused on the Derry Green Corporate Business Park Secondary Plan Area will also apply as outlined in the FSEMS and CFCP. No amendment to the Secondary Plan shall be required to implement the recommendations of the Subwatershed Update Study, FSEMS or CFCP. Such functional recommendations will be implemented through the required SIS as approved by the Town in consultation with Conservation Halton, the Region and any other relevant public agencies. In particular, where the SUS, FSEMS, or CFCP supports the realignment, relocation, elimination, replication or other modification of watercourse corridors or other key features including wetlands and restoration areas or changes in the locations of drainage facilities in accordance with the policies of Section C.9.5.10 of this Plan, no amendment shall be required to this Plan where such works are undertaken.*

*C.9.4.5.3 Subwatershed Impact Studies are required for Subwatershed Impact Areas identified as part of the FSEMS as a submission requirement for a complete application. The study areas can be modified or consolidated subject to approval of the Town, in consultation with the Conservation Authority and the Region of Halton. The goal of the Subwatershed Impact Studies will be to achieve a greater level of detail in the integration of land use, servicing and stormwater management. The Terms of Reference for the Subwatershed Impact Studies must be adhered to and can be found in the FSEMS.*

The Subwatershed Impact Study (SIS) is a thorough and detailed study that is conducted to support a proposed development. One of the crucial results of this study is to determine the alignment of both NHS channel blocks and the road network. The SIS plays a significant role in evaluating and comprehending the potential impact of the development on the local subwatershed. It also helps to align these critical elements accordingly. The SIS that pertains to the subject lands are now approved by the Town.

#### **C.9.4.6 URBAN DESIGN**

##### **C.9.4.6.2 General Design Strategy and Guidelines**

*Further to, and in accordance with the provisions of Vision and Objectives of subsection C.9.3.1, development shall be designed in accordance with the following general design principles:*

- a. Creation of a visually and physically well connected Corporate Business Park incorporating high quality streetscape design and urban design which is innovative and supportive of transit and active transportation;*
- b. Creation of a unique, place specific Corporate Business Park as a gateway to the Urban Area.*

#### **C.9.4.6.4 Natural Heritage System**

*The Natural Heritage System as designated on Schedules “C.9.A.” and “C.9.B” consisting, watercourse corridors and buffers, is a central feature of the Secondary Plan area and forms a strong connection to lands within the Greenbelt Plan.*

#### **C.9.4.6.7 Enhanced Streetscape Design**

*James Snow Parkway and Derry Road represent major access routes into and through the community. In addition, each of these roads has a significant role in the community. Recognizing that both these roads are Regional Roads, the Town will work to ensure that they are designed with an enhanced and coordinated approach to landscaping, street tree planting, sidewalks, lighting, private/public utilities, bike and multi-use paths and boulevards in accordance with the Town’s Urban Design Guidelines and Regional Right-of-Way Dimension Guidelines and subject to any required Environmental Assessment.*

*In addition, the Town shall through the zoning by-law and site plan approval process, control development along these roads to ensure both a high quality of site design and use. In particular:*

- a. buildings shall be designed to front on these roads;*
- b. significant parking areas will be screened by berms and landscaping;*
- c. service, loading and open storage facilities shall not be permitted in the yard abutting these major roads and shall be screened from these roads;*
- d. safe and functional vehicular and pedestrian access shall be provided; and,*
- e. the use of native and non-invasive species for landscaping and street tree planting will be promoted. Consideration will be given to eco-friendly features, including Low Impact Development (LID) practices in any streetscape design and landscaping involving publicly owned roads rights-of-way.*

#### **C.9.4.6.8 Streetscape Design**

*The design of all streets in the Corporate Business Park will also be carefully reviewed to ensure that the streets are functional for goods movement, while still being comfortable for pedestrians and bicyclists. The Urban Design Guidelines will establish specific design treatment for all streets.*

#### **C.9.4.6.10 Gateways**

*“Gateways” are designated on Schedule C.9.A. and are key points of entry to the Urban Area of the Town which require special design treatment of both the road allowance and adjacent development. In addition, it is recognized that the railway also serves as a “gateway” to the Town, and consideration will be given to the screening open storage along the tracks to minimize visual impact.*

The proposed development has undergone a thorough design process that conforms to the urban design policies outlined in Section 9.4.6. The objective is to create an industrial Draft Plan of Subdivision that not only meets functional requirements but also exemplifies high quality and visual interest. The design considerations take into account both the functionality of the industrial space and the aesthetic aspects to create a well-integrated and visually appealing development.

Although the proposed development includes parking situated between the buildings and Derry Road, measures have been taken to enhance landscaping and provide additional setback distances along Derry Road. This strategic approach serves to screen the parking area, aligning with Enhanced Streetscape Design policies. The emphasis on landscaping and setbacks not only contributes to the visual appeal of the development but also aligns with the goal of creating an aesthetically pleasing and pedestrian-friendly streetscape along Derry Road. For more details, please refer to the Urban Design Brief.

As illustrated on **Figure 3** - Conceptual Site Plan, careful consideration has been given to the location of servicing and loading areas for the warehouse buildings. Importantly, these areas are not situated along the Derry Road frontage, contributing to a cleaner and more visually appealing presentation along this major road. The development has been designed to maximize and benefit from views to the NHS, reflecting a commitment to integrating with and respecting the surrounding natural environment.

Efficient access points for both vehicles and pedestrians have been integrated into the site design. This consideration ensures that the development not only functions effectively but also enhance overall accessibility and connectivity. For a more in-depth understanding of the proposed site design, the Urban Design Brief, prepared in support of the application, provides further details on the design principles and strategies employed.

The proposed development's design is driven by a commitment to both functional efficiency and aesthetic excellence. By aligning with urban design policies, enhancing landscaping, carefully placing servicing areas, and maximizing views to the NHS, the development aims to be a harmonious addition to the area, meeting the needs of industry while respecting the visual and environmental context of the site.

#### **C.9.5.1.2 Overlay Designations**

*The following uses shall be the only uses permitted on lands designated “Business Park Area” on Schedule “C.9.B”, in the “Gateway Area”, “Natural Heritage Oriented Area” and “Street Oriented Area” overlay designations, subject to the applicable policies of Sections C.9.5.1.4, C.9.5.1.5 or C.9.5.1.6 and submission of a development plan which illustrate how the policies of this Plan and the Urban Design Guidelines will be addressed:*

- a. Offices;*
- b. Light industrial uses including a significant office component;*
- c. Research and development uses excluding uses which produce biomedical waste;*
- d. Restaurants and service commercial uses where such uses form part of a light industrial or office building or mixed use building, including an industrial mall;*
- e. Commercial recreation uses and public indoor recreation facilities; and,*
- f. Parks.*

The current regulations allow only restaurants and service commercial uses as part of light industrial, office, or mixed-use buildings, including an industrial mall. However, the proposed LOPA aims to permit standalone commercial/retail uses in the “Street Oriented Area” overlay designations. This change will improve the functionality of the industrial subdivision and offer convenient and accessible shopping and dining options for the nearby employment area, aligning with the 'complete community' principle. The design of the building and site layout will be carefully crafted to enhance the streetscaping along Derry Road, contributing positively to the visual appeal of the public realm.

Since the remnant parcel within the employment area is unsuitable for industrial buildings, introducing commercial uses is a practical solution. This approach is seen as a better alternative to expanding parking along Derry Road, promoting a more balanced and diversified land use that aligns with the objectives of the Secondary Plan.

#### **C.9.5.1.7 Natural Heritage Oriented Area**

*Lands designated “Business Park Area” with a “Natural Heritage Oriented Area” overlay designation on Schedule “C.9.B” shall be reviewed by the Town in accordance with the Urban Design Guidelines. In particular:*

- a. Development shall be designated to ensure that buildings and other uses not only capitalize on their location, but that they are sensitive to impacts on the surrounding Natural Heritage System;*
- b. Careful consideration shall be given to the design of impervious surfaces and other factors that impact stormwater management including the use of LID practices;*
- c. Natural heritage supporting areas and restoration areas which may be located on the site and which have been identified through the SUS and FSEMS shall be considered for integration into the development where feasible in such a way as to link them to the Natural Heritage System; and,*
- d. Development shall be designed with a campus like design to achieve better integration with the surrounding Natural Heritage System.*

The proposed development has been designed with a strong emphasis on protecting and enhancing the NHS. To achieve this, the realignment of NHS channel blocks has been guided by both the FSEMS and the SIS. These studies have been essential in determining the best alignment to ensure a thoughtful and ecologically sensitive approach to development.

As shown on the conceptual site plan, the warehouse buildings have been strategically positioned with appropriate setbacks from key features of the NHS. This design decision has been intentional to minimize the impact on sensitive natural areas while also taking advantage of scenic views. The proposed development seeks to balance industrial functionality and environmental preservation by incorporating setbacks.

The Urban Design Brief and SIS are additional resources that provide a more comprehensive understanding of the contextual choices made in the design process. These documents outline the specific strategies employed to enhance connectivity and preserve the NHS lands on the subject property. By detailing the decision-making process, these briefs provide transparency regarding the development's commitment to environmental stewardship and the integration of natural features into the overall design.

The proposed development has been carefully designed to be environmentally responsible, with a focus on protecting and enhancing the NHS. The realignment of channel blocks, appropriate setbacks, and contextual choices outlined in the accompanying Urban Design Brief and SIS aim to set an example for sustainable and conscientious land use, contributing positively to both the local ecology and the overall community.

#### **C.9.5.4 NATURAL HERITAGE SYSTEM**

##### **C.9.5.4.1 Purpose**

*Within the Derry Green Corporate Business Park Secondary Plan, the “Greenlands A Area”, and “Greenlands B Area” designations as established in the Official Plan are collectively designated “Natural Heritage System” in order to better reflect the systems approach taken to ensure the protection, preservation and enhancement of the key features, buffers and linkages of which it is composed. The purpose of the Natural Heritage System designation in the Derry Green Corporate Business Park Secondary Plan Area is:*

- a. to protect areas which have been identified as having environmental significance based on the functional recommendations of the Sixteen Mile Creek Areas 2 and 7 Subwatershed Update Study, the FSEMS, and CFCP for the Derry Green Corporate Business Park Secondary Plan Area; and,*
- b. to establish a Natural Heritage System, achieving enhanced natural habitat areas and ecological functions that will be resilient to the impacts of the adjacent urban development.*

##### **C.9.5.4.3 Permitted Uses**

*The Natural Heritage System designation on Schedules “C.9.A” and “C.9.B” means that only the following uses may be permitted subject to the policies of this Section:*

- a. recreational trails and similar non-intensive recreation uses;*
- b. forest, wildlife and fisheries management;*
- c. archaeological activities in accordance with Provincial Ministry requirements; and,*
- d. public infrastructure in accordance with Section C.9.4.8.*

##### **C.9.5.4.5 Criteria for Watercourse Corridor Design**

*The FSEMS and CFCP identify the approach for watercourse corridor design to be followed in the SIS. The dimensions of watercourse corridors, exclusive of the required buffers set out in the C.9.5.4.6, measured from stabled top of bank to stable top of bank, shall be determined in accordance with the recommendations of an approved SIS on the basis of the following criteria:*

- a. Meander belt width for natural channel design, including 10% safety factor, and all required fisheries compensation/habitat;*
- b. Maintenance of existing riparian storage volumes;*
- c. Watercourse corridors designated to contain Region Storm storage shall be sized accordingly;*
- d. Construction of a stable valley wall from the toe of the valley wall to the proposed finished grade at the top of valley wall; and,*
- e. Provision of flood protection for adjacent properties up to and including the Regional Storm event.*

#### **C.9.5.4.6 Criteria for Buffers**

*Buffers shall be provided in accordance with the following:*

- a. *Watercourse Corridors:  
10 metres from the greatest hazard (Regional Storm flood plain or stable top of bank), except where a trail is planned to be located within the buffer, in which case an additional 5 metres will be added to the buffer width and the trail will be located within the outer 5 metre portion of the buffer in accordance with Schedule C.9.A. Notwithstanding the foregoing, where a trail is located within a stormwater management facility adjacent to a watercourse buffer, an additional 5 metre buffer width will not be required;*

#### **C.9.5.4.7 Natural Heritage Systems Policies**

- a. *The boundaries of the Natural Heritage System designations on Schedules “C.9.A.” and “C.9.B” have been delineated in a conceptual manner based on the functional recommendations of the Sixteen Mile Creek, Subwatershed Planning Study, Areas 2 and 7, Subwatershed Update Study and FSEMS for the Derry Green Corporate Business Park Secondary Plan Area. These boundaries are subject to field verification as part of the preparation of the Subwatershed Impact Studies, and within those areas regulated by Conservation Halton must be consistent with the policies of Conservation Halton.*
- b. *Where as part of the subwatershed planning process or in the FSEMS and the approval of the Subwatershed Impact Study(s) it is recommended that the boundary of the Natural Heritage System can be altered through the removal, restoration and/or modification or realignment of watercourse corridors, or other features, or the relocation, modification or elimination of restoration areas, or the modification or elimination of wetlands subject to the replication of their function elsewhere in the Natural Heritage System within the Derry Green Corporate Business Park Secondary Plan, and the Town approves the recommendation, in consultation with Conservation Halton, the recommended alteration of the Natural Heritage System boundary can be made without further amendment to this Plan where such works are undertaken in accordance with an approved SIS. Furthermore, the Natural Heritage Systems designation shall be deemed to apply to all lands within the modified boundaries and an adjacent land use designation shall be deemed to apply to any lands removed from the Natural Heritage designation;*
- c. *Endangered and threatened species were identified in the Derry Green Corporate Business Park Secondary Plan Area through the Subwatershed Update Study. Prior to the earlier of the site alteration, subdivision registration and/ or site servicing, the proponent will be required to address impacts, if any, to endangered and threatened species through consultation with the Ministry of Natural Resources;*
- d. *The lands within the Natural Heritage System designation are considered to be a crucial part of the proposed Natural Heritage System and open space system intended for the Milton Urban Area and shall be acquired by the Town of Milton in accordance with the policies of Section B.5.9.3.7 of this Plan. However, where any land within the Natural Heritage System designation is held under private ownership, this Plan shall not be construed as implying that such areas are free and open to the general public nor that such*

*lands shall be purchased by the Town or other public agency, but may be required by dedication through the development approval process; and; e. Lands within the Natural Heritage System designation shall be enhanced and restored in accordance with the recommendations of the approved FSEMS, CFCP and SIS. Any requirements for restoration plantings shall be in accordance with Town standards as set out in the FSEMS.*

The proposed development shows a strong commitment towards protecting the lands that are designated as the NHS by creating realigned channel blocks. These channel blocks are designed in a way that no construction activity will be allowed, which reflects the understanding of the ecological importance of these areas and a commitment to their preservation.

One of the key highlights of these realigned channel blocks is that they incorporate varying widths, which has been done strategically to ensure the continued functionality of the tributaries of the Sixteen Mile Creek. By accommodating varying widths, the development aims to respect the unique characteristics of these water features, contributing to the overall health and functionality of the local water system.

Apart from the varying widths, a 10-meter buffer is also included in the design of the channel blocks. The buffer zone serves multiple purposes, including environmental protection and compliance with the findings of the SIS and the FSEMS. It acts as a protective barrier, ensuring a degree of separation between development and the sensitive Natural Heritage System lands, in accordance with established environmental management guidelines.

The alignment of the channel blocks, combined with the varying widths and the incorporation of a 10-meter buffer, collectively represents a holistic approach to land use planning. It demonstrates a commitment to environmental conservation, recognizing the significance of tributaries and the broader ecological system associated with the Sixteen Mile Creek. This approach aligns with best practices in sustainable development and ensures that the proposed development is not only functional but also environmentally responsible.

#### **C.9.5.6 ENVIRONMENTAL LINKAGE AREA**

*Further to, and in accordance with, the policies of Section B.3.13, Environmental Linkage Area, of the Official Plan, the following policies apply to the Environmental Linkage Area designation on Schedule “C.9.B” which consists of the local scale linkage components of the Natural Heritage System identified through the Subwatershed Update Study, Areas 2 and 7, FSEMS and an approved Subwatershed Impact Study:*

- a. The Environmental Linkage Area designation is shown conceptually on Schedule “C.9.B”. This designation is designed to protect lands which have the potential to form linkages in the Natural Heritage System. The designation includes in some cases existing features such as hedgerows, intermittent flow channels or overland flow routes, which have the potential to be rehabilitated or enhanced to form the basis for the linkage, while recognizing that these areas would not generally be subject to any corridor or buffer requirements. They may also be areas where there are no existing features but where trails or landscape areas would be created to form a linkage;*
- b. Notwithstanding the permitted uses in Section B.3.13.2, parking lots and recreational trails are prohibited;*

- c. *It is the objective of the Town to have the lands in the Environmental Linkage Area designation, where their protection is recommended through the approved Subwatershed Impact Study, dedicated to the Town at no cost to the Town; The Environmental Linkage Area has been conceptually identified on the subject lands, directly south of the Street Oriented Area and Gateway Area overlays. An SIS has been prepared in support of the proposed development and determined that the creation of an environmental linkage in this area is not required.*

### **C.9.5.9 STORM WATER MANAGEMENT FACILITY/LOW IMPACT DEVELOPMENT PRACTICES**

#### **C.9.5.9.1**

*The Stormwater Management Facility designation on Schedules “C.9.A” and “C.9.B” represents a general location for these facilities. The location and configuration of the Stormwater Management Facilities shall be more specifically delineated in the FSEMS. They will be further refined through the applicable Subwatershed Impact Studies and through Stormwater Management Plans prepared in support of individual development applications. Through the preparation and review of these studies and plans, careful consideration shall also be given to the use of low impact development (LID) practices for stormwater management including the design of impervious surfaces and other factors that positively impact on stormwater management. Through these studies, the management of stormwater from public property, including Regional roadways, shall also be accommodated.*

#### **C.9.5.2**

*Stormwater Management Facility sites may be relocated or consolidated without an amendment to this Plan, subject to the approval of the Town and relevant agencies, provided alternative sites are consistent with the goal, objectives and policies of this Secondary Plan and the Sixteen Mile Creek, Subwatershed Update Study, Areas 2 and 7, 2010, FSEMS, Conceptual Fisheries Compensation Plan and Subwatershed Impact Studies.*

The proposed development incorporates a comprehensive Stormwater Management strategy, including the provision of one SWM pond and two SWM detention basins strategically located within the site. The SWM features are designed to control and manage urban runoff effectively, mitigating potential impacts on both the development site and the extended Clark Boulevard.

Specifically, one SWM pond is situated on the east side of the extension of Clark Boulevard, south of block 2, and abuts the NHS channel block. This location is likely chosen for its strategic position to efficiently manage stormwater runoff from the development and the adjacent road network.

Additionally, two SWM detention basins are proposed at the rear of blocks 1 and 3. These basins are strategically positioned to address stormwater management needs associated with specific areas within the development.

It is worth noting that the initial plan for the Derry Green Corporate Business Park Land Use (Schedule C.9.B, **Figure 8**) suggested six possible locations for stormwater management (SWM) ponds. However, a detailed analysis carried out through the FSR revealed that only one SWM pond and two SWM detention

basins were necessary. This decision was likely based on a comprehensive assessment of factors such as site topography, soil conditions, and drainage patterns.

By optimizing the number and location of SWM features, the proposed development aims to efficiently control and minimize urban runoff. This approach is in line with the best practices in stormwater management and reflects a commitment to environmentally responsible development. It ensures that potential impacts on the surrounding environment, including the NHS, are effectively mitigated.

## **C.9.6 PHASING AND FINANCE**

### **C.9.6.1.2**

*In addition to the policies of Section C.9.6.1.1, development in the Derry Green Corporate Business Park shall proceed in three phases as designated on Schedule “C.9.C”, as well as a Potential Future Phase Prior to the commencement of development in each phase, the policies of Section C.9.6.1.1 shall be satisfied and confirmation shall be received from the Regional Municipality of Halton that water and wastewater services can be provided, and the following conditions must be met:*

- a. Phase 2 - 60% of the developable land in Phase 1 must be in registered plans of subdivision or approved site plans prior to commencement of development in Phase 2;*
- b. Phase 3 - 60% of the developable land in Phase 2 must be in registered plans of subdivision or approved site plans prior to commencement of development in Phase 3; and,*
- c. Potential Future Phase – the Town and Regional Municipality of Halton are satisfied that the lands can be appropriately provided with water and wastewater services.*

*Notwithstanding, the policies in Section C.9.6.1.2 and the phasing shown on Schedule “C.9.C”, the phasing may change as a result of the approval and the implementation of the Long-Term Fiscal Impact Assessment of the Growth dated December 6, 2010 prepared by Watson & Associates Economists Ltd. as modified by Report CORS – 063-12 or other considerations including the timing of servicing availability in accordance with the policies set out in Section C.9.6.1.1; and Section C.9.6.1.2 and Schedule “C.9.C” can be modified by the Town without amendment to this Plan.*

The subject lands are positioned within Phase 2 of the Derry Green Corporate Business Park (DGCBP) Phasing Plan, as outlined on Schedule C.9.C - Derry Green Corporate Business Park Phasing Plan (**Figure 10**). The delineation of phases is a crucial aspect of the planning process, ensuring a systematic and coordinated approach to the development of the Derry Green area.

The proposed development is committed to meeting the applicable conditions for development as per the DGCBP Secondary Plan policies. This commitment includes the confirmation that adequate waste and wastewater servicing can be provided. Ensuring sufficient waste and wastewater infrastructure is crucial for the sustainable and responsible development of the area, aligning with the broader goals outlined in the DGCBP Secondary Plan.

By being positioned within Phase 2 and conforming to the policies and conditions outlined in the DGCBP Secondary Plan, the proposed development contributes to the orderly and well-planned growth of the Derry Green Corporate Business Park. The phased approach allows for careful coordination of infrastructure,

services, and amenities, ensuring that the development aligns with the overall vision for the area while meeting the needs of the community and stakeholders.

In summary, the location of the subject lands within Phase 2 signifies a specific timeline within the larger development plan for the DGCBP. The commitment to satisfying applicable conditions, particularly in terms of waste and wastewater servicing, reflects a dedication to responsible and phased development per the policies outlined in the DGCBP Secondary Plan.

The proposed development conforms with the policies outlined in the DGCBP Secondary Plan for several compelling reasons:

**High-Quality Employment Lands and NHS Connectivity:**

The proposed development meets the core intent of the DGCBP Secondary Plan by offering high-quality employment lands strategically sited to enhance the overall connectivity of the NHS.

**Protection of NHS and Natural Heritage Oriented Area:**

The development prioritizes the protection of lands designated as NHS and within the Natural Heritage Oriented Area. This is achieved through the creation of NHS channel blocks, where no development will be permitted, along with the incorporation of appropriate building setbacks. These measures contribute to preserving and enhancing the natural features of the area.

**Conformance to Business Park Designation:**

The proposal adheres to the Business Park designation by providing three distinct business park blocks, each accommodating a warehouse and accessory office uses. This conforms with the intended land use outlined in the DGCBP Secondary Plan.

**Gateway Area Overlay Compliance:**

The development conforms to the Gateway Area Overlay designation by facilitating the construction of a warehouse building with office uses along Derry Road. The building meets specific criteria, including a minimum height of 2 storeys and the incorporation of predominant architectural features, contributing to the visual character of the gateway.

**Optimized Industrial Uses and Streetscape Design:**

The proposed Official Plan Amendment, allowing limited parking between the building and Derry Road within the Street Oriented Area, demonstrates a thoughtful approach. This design choice maximizes the effectiveness and efficiency of industrial uses while utilizing landscaping to minimize the visual impact of parking on the streetscape.

**Phasing Plan Adherence:**

The development conforms with the designated phase within the DGCBP Phasing Plan. Adherence to this plan ensures a systematic and coordinated approach to development, meeting the conditions set forth for each phase.

**Job Creation and Employment Density:**

The proposed development is estimated to create approximately 1700 jobs, demonstrating alignment with employment density calculations from Halton Region's Investment Readiness and Employment Study. This contributes significantly to the employment goals outlined in the DGCBP Secondary Plan.

Enhanced Transportation Connectivity:

Through coordination with the landowner to the south, the proposed development contributes to transportation connectivity by extending Clark Boulevard to Derry Road. This extension enhances overall connectivity within the area, aligning with broader transportation objectives.

In summary, the proposed development conforms to the policies of the DGCBP Secondary Plan (other than parking along Derry Road and stand-alone commercial/retail permission on a block adjacent to Derry Road) reflects a comprehensive and well-planned approach, considering environmental, economic, and infrastructure considerations outlined in the regional planning framework.

## **5.0 Proposed Zoning By-law Amendment**

The subject lands are currently included in the Urban Zoning By-law (Zoning By-law No. 016-2014) and Rural Zoning By-law (By-law No.144-2003). Under Zoning By-law No. 016-2014, it currently carries the zoning designations of 'Golf Course Zone (GC)' and 'Natural Heritage System (NHS)' as per Section 11.1 of the Town of Milton Zoning By-law 016-2014. Under By-law No.144-2003 it carries the zoning designation Greenlands (A). However, the proposed development, which includes industrial uses, is not permitted under the current zoning regulations.

A Zoning By-law Amendment is necessary to enable the intended development. This amendment would involve modifying the existing zoning regulations to allow for the inclusion of industrial and commercial/retail uses on the subject lands. Additionally, Zoning By-law No.144-2003 (Rural) applicable to the subject lands will be repealed to include the full extent of the lands in the Urban Zoning By-law 016-2014.

The Zoning By-law Amendment process typically involves a series of steps, including public consultations, and municipal approvals, and may also require an examination of the potential impacts on the surrounding community and environment. By seeking this amendment, the proponent aims to bring the zoning regulations in line with the proposed land use, creating a legal framework for the development to proceed.

The requirement for a Zoning By-law Amendment stresses the importance of aligning proposed land uses with existing zoning regulations to ensure consistency and compliance with the municipality's planning policies. This process facilitates transparent decision-making and allows for public input, contributing to a well-informed and inclusive approach to urban development.

The proposal seeks to amend the Town of Milton Zoning By-law 016-2014 to rezone the subject lands to a site-specific 'Business Park (M1\*XXX)', site-specific 'Local Commercial (C3\*XXX)', 'Open Space (OS-2)' for the Stormwater Management Pond, and 'NHS' zone to implement the DGCBP Secondary Plan and permit the proposed development. The proposed site-specific M1 zone includes a reduction of the number of required parking spaces via a parking ratio as supported by a Parking Justification Study. The draft Amending Zoning By-law is appended to this as Appendix II.

The proposal aims to amend the existing zoning regulations outlined in the Town of Milton Zoning By-law 016-2014 for the subject lands. The proposed rezoning will align with the DGCBP Secondary Plan and facilitate the intended development. The proposed amendments are as follows:

**Business Park (M1\*XXX) Zone:**

The rezoning seeks to establish a site-specific 'Business Park (M1\*XXX)' zone for the subject lands. This designation is tailored to accommodate industrial and business-related uses with a specific parking ratio for the industrial and associated office uses.

A universal parking ratio of 1 space per 223 m<sup>2</sup> is proposed for all three Business Park Blocks, the reduction in parking has been justified as part of the TIS. We are confident that the proposal will comply with all other zoning regulations of the parent zoning by-law.

**Local Commercial (C3\*XXX) Zone:**

The rezoning seeks to establish a site-specific 'Local Commercial (C3\*XXX)' zone. The site-specific regulations will accommodate a larger maximum lot area, and larger maximum gross floor area for the combined and individual buildings.

A maximum lot area of 7500m<sup>2</sup>, a maximum Gross Floor Area for all Buildings Combined 1900m<sup>2</sup>, and a maximum Gross Floor Area for Individual Buildings of 1200m<sup>2</sup>.

**Open Space -2 (OS-2) Zone for Stormwater Management Pond:**

An 'Open Space (OS-2)' zoning designation is proposed to accommodate the Stormwater Management Ponds and required pond buffers. This zoning recognizes the importance of dedicated open space for stormwater management, ensuring compliance with environmental and land use regulations.

**NHS Zone:**

The proposed rezoning includes maintaining the 'Natural Heritage System (NHS)' zoning to preserve and protect the designated natural areas on the subject lands. This zone includes the Channel/Floodway, NHS Buffers, and a Woodlot. This aligns with the environmental conservation goals outlined in the DGCBP Secondary Plan.

The site-specific M1 zone includes a reduction in the number of required parking spaces. This adjustment is supported by a Parking Justification Study, indicating that the proposed parking ratio is suitable for the development's needs. This flexibility in parking requirements acknowledges the specific characteristics and requirements of the proposed business park development. The site-specific C3 zone allows for a larger maximum lot area, and larger maximum gross floor area for the combined and individual buildings. The increase in size will allow for a better-functioning block that can accommodate parking and more functional commercial and retail space.

The draft Amending Zoning By-law, appended to the proposal as Appendix II, provides the detailed legal framework for these zoning amendments. This document outlines the specific regulations and conditions that will govern land use, setbacks, parking ratios, and other relevant aspects to facilitate the proposed development per the DGCBP Secondary Plan.

The proposed rezoning conforms with the broader planning objectives, providing a regulatory framework that supports both economic development and environmental conservation in the Derry Green Corporate Business Park.

## **6.0 Proposed Official Plan Amendment**

To allow the proposed development of an industrial subdivision, amendments to the Derry Green Corporate Business Park Secondary Plan are required. The proposed amendments are as follows:

- 4.11.3.XX The land identified as Specific Policy Area No. XX on Schedule II of this Plan, being the lands at 6728 Sixth Line are permitted to allow surface parking between the building and the street, and standalone commercial/retail uses within the Street Oriented Business Park overlay designation on the south side of Derry Road between Fifth and Sixth Line.*

As outlined in the Urban Design Brief and landscape plans, the proposed amendment is shaped to conform seamlessly with the overarching vision and intent of the Secondary Plan. The design prioritizes ensuring that parking remains subtle and not prominently visible from Derry Road. This is achieved through generous setbacks and the integration of landscaping elements, underscoring a dedicated commitment to harmonizing with the urban design of the surrounding streetscape. The detailed consideration of these components is aimed at preserving the aesthetic character of the area and minimizing any potential visual impact, per the broader objectives outlined in the Secondary Plan.

In addition to the industrial subdivision, there are also commercial/retail uses included in the design. This addition complements the functionality of the subdivision and serves the local employment area. It conforms with the principle of a 'complete community' by offering convenient and accessible shopping and dining options for the nearby workers. The building and site layout are carefully designed to enhance the streetscape along Derry Road, which contributes positively to the visual appeal of the public area.

The location of this development on a remnant parcel within the employment area, which is otherwise unsuitable for industrial building, underscores the practicality of introducing commercial uses. This approach is seen as a preferable alternative to expanding parking along Derry Road, contributing to a more balanced and diversified land use that conforms with the objectives of the Secondary Plan.

The proposed draft OPA can be found in Appendix I.

## **7.0 Concluding Planning Opinion**

The proposed Official Plan Amendment, Zoning Bylaw Amendment, and Draft Plan of Subdivision are well-supported by good planning principles.

1. **Consistency with Provincial and Regional Plans:**  
The proposal is in harmony with the Provincial Policy Statement and aligns with the Provincial Growth Plan for the Greater Golden Horseshoe, the Region of Halton Official Plan, and the Town of Milton Official Plan. This consistency ensures that the development aligns with broader regional and provincial planning objectives.
2. **Conformance with DGCBP Secondary Plan:**  
The proposal adheres to the majority of policies outlined in the Derry Green Corporate Business Park Secondary Plan, except for a limited parking provision between the buildings and Derry Road and to allow stand-alone commercial uses in the Street Oriented Business Park overlay. These exceptions are justifiably made to optimize the effectiveness and efficiency of employment uses.
3. **Consistency with Provincial and Regional Policies:**  
The proposed amendment to the DGCBP Secondary Plan to allow limited parking between the building and Derry Road and to allow standalone commercial/retail uses is consistent with provincial and regional policies that encourage maximizing the efficiency of employment uses.

4. **Job Creation and Employment Density:**  
The development is estimated to create approximately 1700 jobs, contributing significantly to the achievement of Provincial, Regional, and Town employment targets. This aligns with broader economic development goals.
5. **Support for Significant Employment Zone:**  
The proposal provides high-quality employment uses within a Provincial Significant Employment Zone, emphasizing its contribution to the long-term economic viability of the surrounding community.
6. **Efficient Infrastructure Use:**  
The development optimizes the use of existing and planned infrastructure, reflecting a commitment to resource efficiency and sustainable development practices.
7. **Stormwater Management and NHS Protection:**  
The provision of a single Stormwater Management (SWM) pond aligns with the Subwatershed Impact Study, demonstrating a commitment to managing urban runoff and minimizing impacts on the Natural Heritage System (NHS).
8. **NHS Protection through Channel Blocks:**  
The creation of NHS channel blocks showcases a commitment to protecting and preserving the Natural Heritage System, contributing to environmental conservation.
9. **Enhanced Views and Vistas of NHS:**  
The proposal's design will enhance the views and vistas of the NHS while actively working to mitigate human impacts, contributing to the overall aesthetic and ecological quality of the development.
10. **Road Network Connectivity:**  
The proposed development integrates seamlessly with existing and planned roads, contributing to the completion of the planned road network and enhancing overall transportation connectivity.
11. **Coordinated Services and Amenities:**  
Coordination with adjacent developments ensures the delivery of adequate services and amenities, promoting a holistic and integrated approach to community development.

*Glen Schnarr & Associates Inc.*

In summary, the proposed amendments and development plans are well-founded in planning principles, demonstrating a thorough consideration of economic, environmental, and community factors. The justifications align with both local and regional planning frameworks, emphasizing sustainability, economic vitality, and responsible land use planning.

Respectfully submitted,

**GLEN SCHNARR & ASSOCIATES INC.**



Colin Chung, MCIP, RPP  
Managing Partner



Herman Wessels, B & TRP (UP)  
Planner

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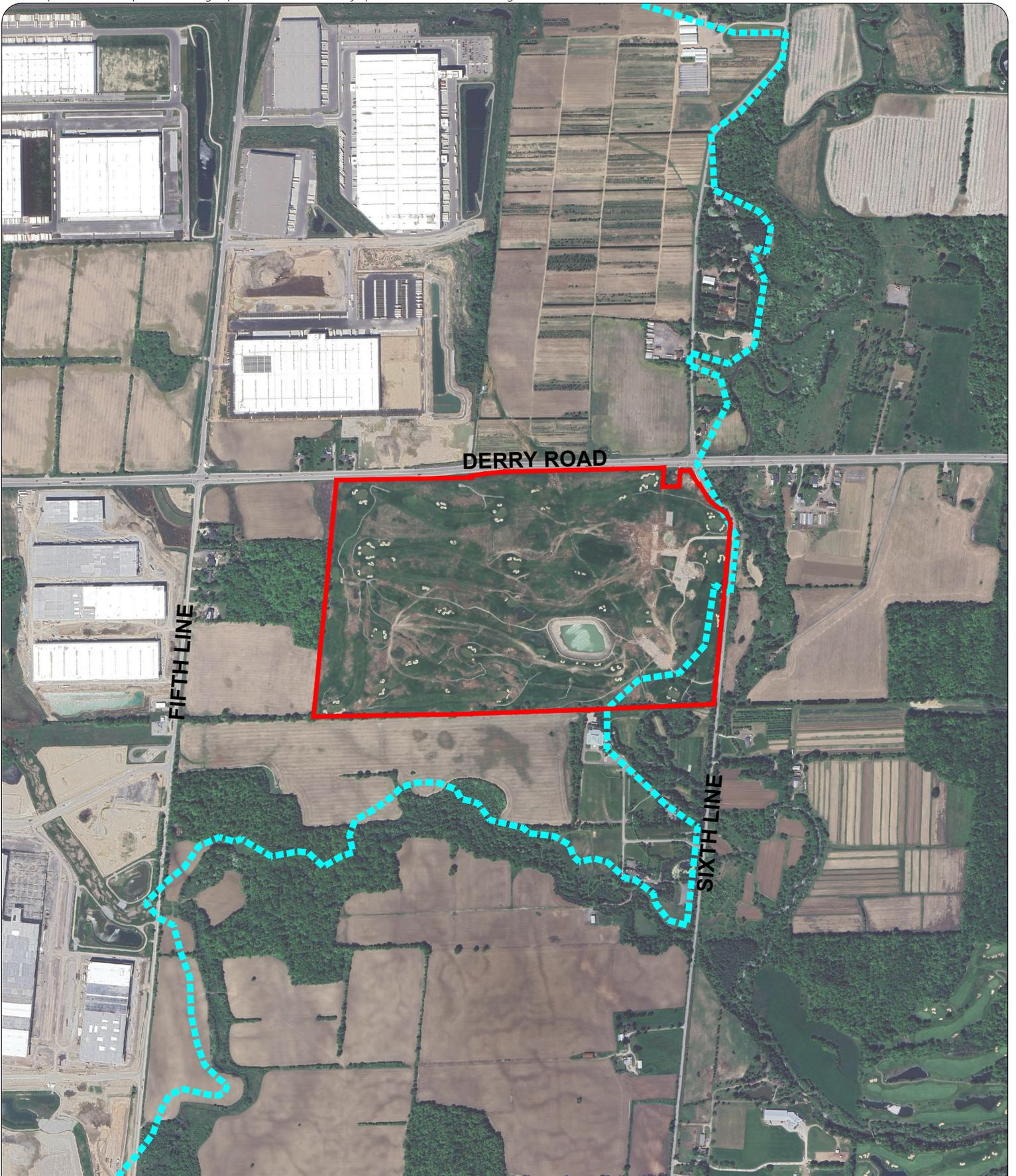


FIGURE  
**1** **AERIAL CONTEXT MAP**  
MILTON, ONTARIO

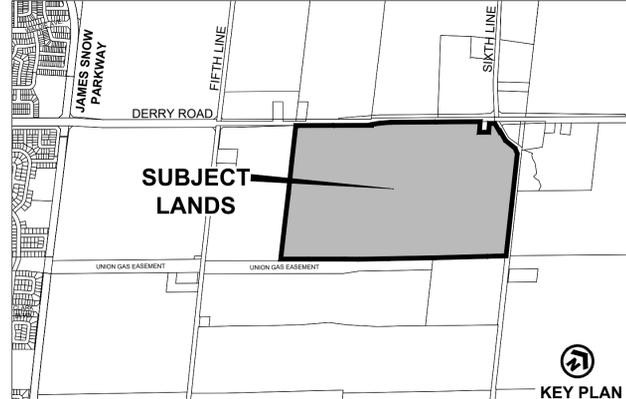
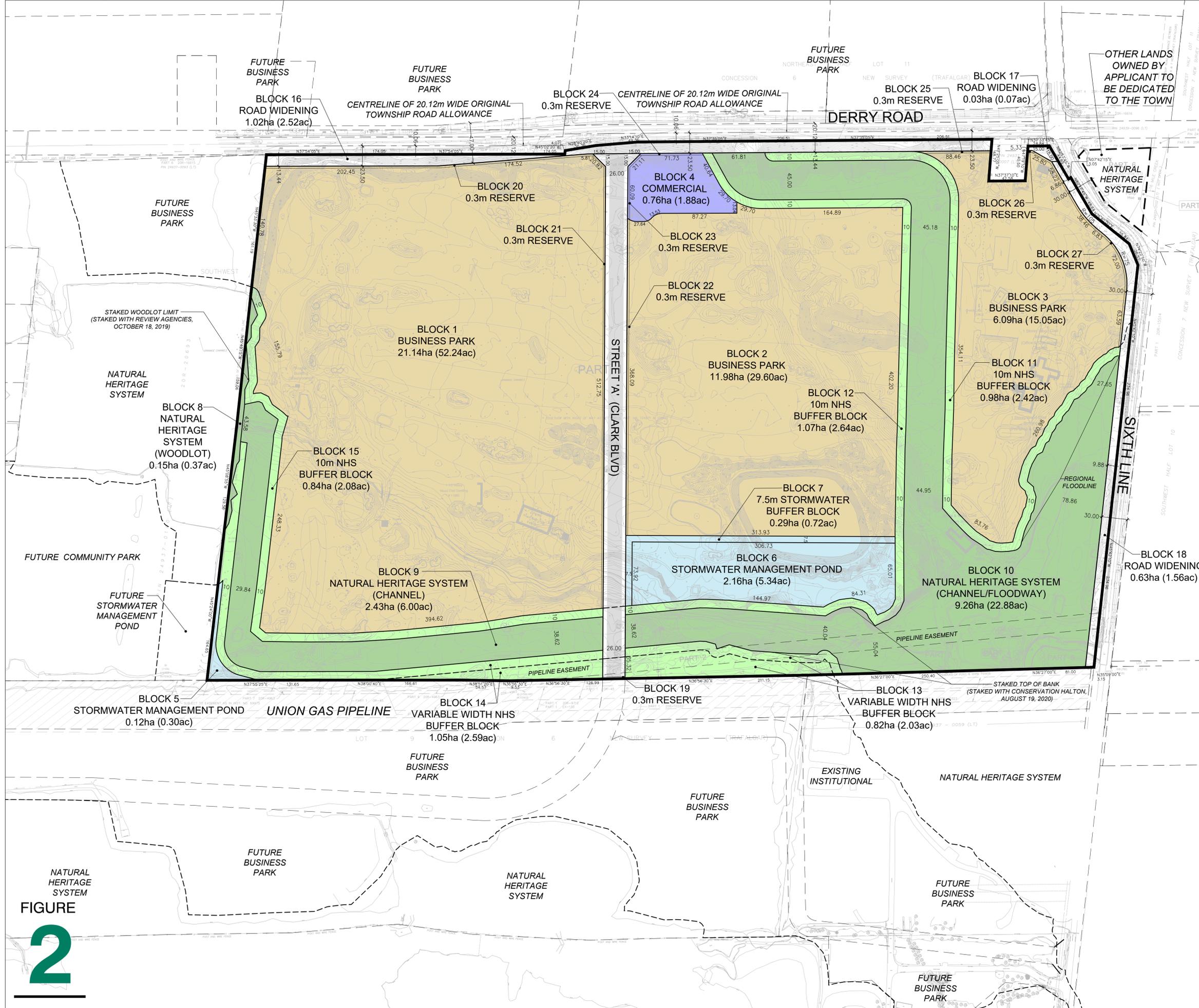
**LEGEND**

- Subject Lands
- Derry Green Corporate Business Park Boundary

6728 SIXTH LINE  
PART OF LOT 10, CONCESSION 6  
TOWN OF MILTON,  
REGIONAL MUNICIPALITY OF HALTON



SCALE NTS  
JANUARY 12, 2024



**DRAFT PLAN OF SUBDIVISION  
ANATOLIA INVESTMENTS CORP.  
FILE # 24T-  
PART OF LOT 10, CONCESSION 6, NEW SURVEY  
TOWN OF MILTON,  
REGIONAL MUNICIPALITY OF HALTON**

**OWNERS CERTIFICATE**  
I HEREBY AUTHORIZE GLEN SCHNARR & ASSOCIATES INC. TO PREPARE AND SUBMIT THIS DRAFT PLAN OF SUBDIVISION TO THE TOWN OF MILTON FOR APPROVAL.

SIGNED *B. Elmaagacli* DATE: NOVEMBER 28, 2023  
BEKIR ELMAAGACLI  
ANATOLIA INVESTMENTS CORP.

**SURVEYORS CERTIFICATE**  
I HEREBY CERTIFY THAT THE BOUNDARIES OF THE LANDS TO BE SUBDIVIDED AS SHOWN ON THIS PLAN AND THEIR RELATIONSHIP TO ADJACENT LANDS ARE CORRECTLY AND ACCURATELY SHOWN.

SIGNED *Robert D. McConnell* DATE: NOVEMBER 11, 2022  
ROBERT D. MCCONNELL  
ONTARIO LAND SURVEYOR

**ADDITIONAL INFORMATION**  
(UNDER SECTION 51(17) OF THE PLANNING ACT) INFORMATION REQUIRED BY CLAUSES A,B,C,D,E,F,G, J & L ARE SHOWN ON THE DRAFT AND KEY PLANS.

H) MUNICIPAL AND PIPED WATER TO BE PROVIDED  
I) CLAY SILT, SAND, GRAVEL  
K) SANITARY AND STORM SEWERS TO BE PROVIDED

**LAND USE SCHEDULE**

LAND USE	BLOCKS	AREA (ha)	AREA (ac)
BUSINESS PARK	1-3	39.21	96.89
COMMERCIAL	4	0.76	1.88
STORMWATER MANAGEMENT POND	5, 6	2.28	5.63
STORMWATER MANAGEMENT POND BUFFER	7	0.29	0.72
NATURAL HERITAGE SYSTEM (NHS)	8-10	11.84	29.26
NATURAL HERITAGE SYSTEM BUFFERS	11-15	4.76	11.76
ROAD WIDENINGS	16-18	1.68	4.15
0.3m RESERVES	19-27	0.06	0.15
26.0m COLLECTOR R.O.W. (610m LENGTH)		1.61	3.98
<b>TOTAL</b>	<b>27</b>	<b>62.49</b>	<b>154.42</b>

**NOTES**  
- COLLECTOR TO ARTERIAL DAYLIGHT TRIANGLE = 15m x 15m  
- PAVEMENT ILLUSTRATION IS DIAGRAMMATIC

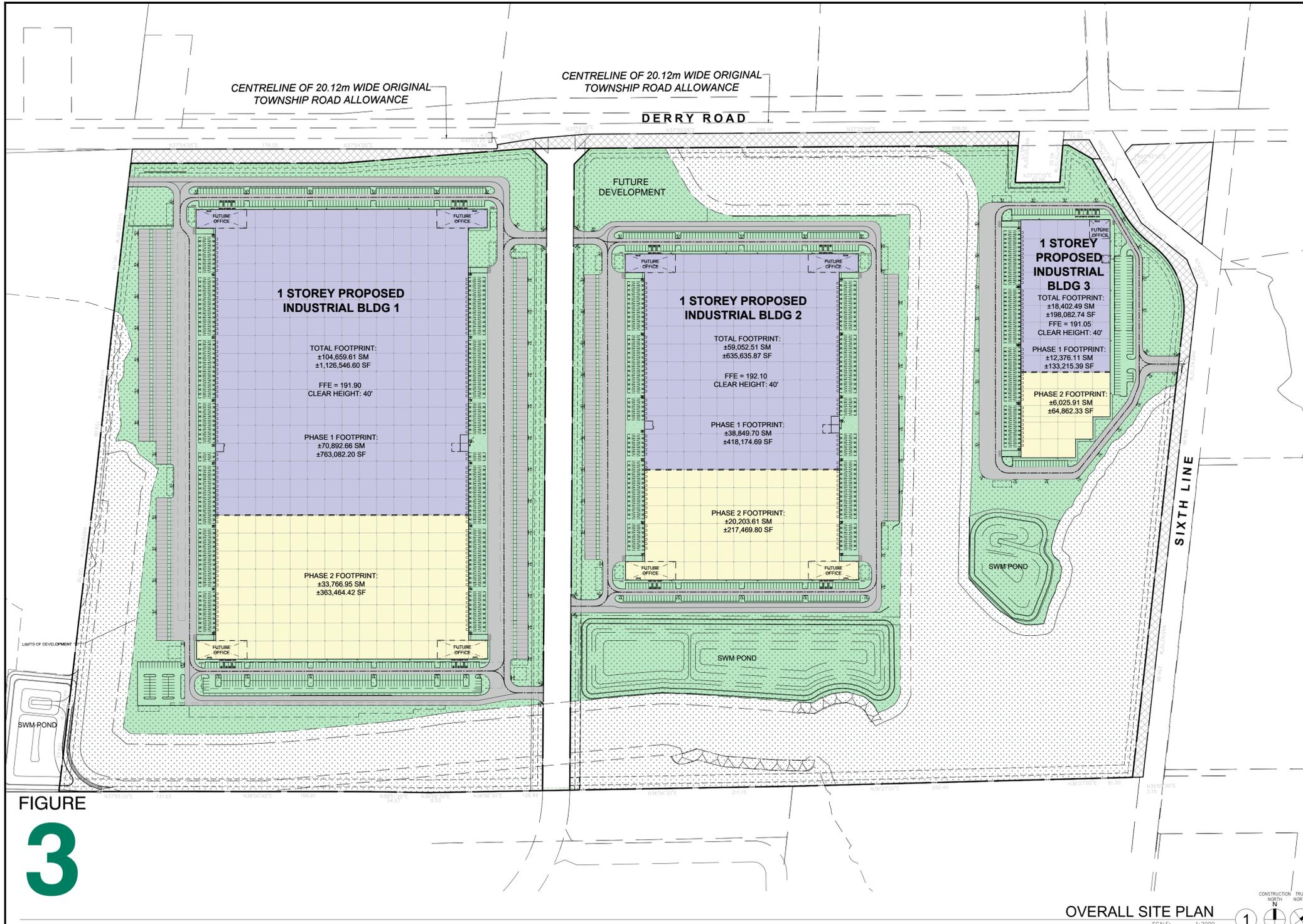


FIGURE 3

**BLDG 1-Zoning Matrix**

Provision	Required (M1 Zone)	Proposed (M1-XX Zone)
Zoning Category	M1 Zone	M1-XX
Lot Frontage (min)	40.0 m	388.53 m
Lot Area (Block 1)	0.80 ha	211,950 m <sup>2</sup>
Gross Floor Area	N/A	104,660 m <sup>2</sup>
Lot Coverage (with municipal services)	No maximum	49.38%
Front Yard Setback (Min)	9.0 m	43.84 m
Interior Side Yard Setback (Min)	3.0 m	66.63 m
Exterior Side Yard Setback (Min)	9.0 m	53.02 m
Rear Yard Setback (Min)	12.0 m	125.58 m
Landscape Open Space (min)	10%	16.85%
Number of Parking Spaces (min)	572	469
Number of Accessible Parking Space (min)	14	16
Number of Loading Spaces (min)	14	155
Bicycle Parking (min)	17	18
Building Height (max)	15 m	13.716 m

**BLDG 2-Zoning Matrix**

Provision	Required (M1 Zone)	Proposed (M1-XX Zone)
Zoning Category	M1 Zone	M1-XX
Lot Frontage (min)	40.0 m	69.45
Lot Area (Block 2)	0.80 ha	154,107 m <sup>2</sup>
Gross Floor Area	N/A	59,053 m <sup>2</sup>
Lot Coverage (with municipal services)	No maximum	38.32%
Front Yard Setback (Min)	9.0 m	100.51 m
Interior Side Yard Setback (Min)	3.0 m	39.97 m
Exterior Side Yard Setback (Min)	9.0 m	49.23 m
Rear Yard Setback (Min)	12.0 m	194.29 m
Landscape Open Space (min)	10%	15.26%
Number of Parking Spaces (min)	344	296
Number of Accessible Parking Space (min)	9	12
Number of Loading Spaces (min)	9	109
Bicycle Parking (min)	10	12
Building Height (max)	15 m	13.716 m

**BLDG 3-Zoning Matrix**

Provision	Required (M1 Zone)	Proposed (M1-XX Zone)
Zoning Category	M1 Zone	M1-XX
Lot Frontage (min)	40.0 m	96.51m(66.05+30.51)
Lot Area (Block 3)	0.80 ha	60,346 m <sup>2</sup>
Gross Floor Area	N/A	18,402 m <sup>2</sup>
Lot Coverage (with municipal services)	No maximum	30.49%
Front Yard Setback (Min)	9.0 m	70.34 m
Interior Side Yard Setback (Min)	3.0 m	40.48 m
Exterior Side Yard Setback (Min)	9.0 m	14.18 m
Rear Yard Setback (Min)	12.0 m	304.87 m
Landscape Open Space (min)	10%	39.60%
Number of Parking Spaces (min)	140	132
Number of Accessible Parking Space (min)	6	6
Number of Loading Spaces (min)	5	51
Bicycle Parking (min)	4	4
Building Height (max)	15 m	13.716 m

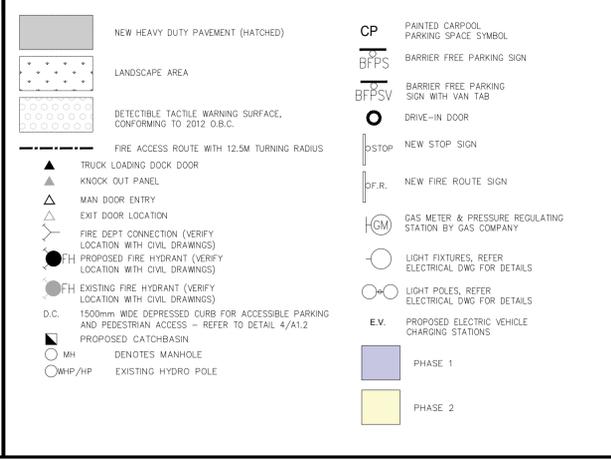
ZBL	Minimum ZBL Requirements	Requirement BLD 1 (M1 Zone)	Proposed BLD 1	Requirement BLD 2 (M1 Zone)	Proposed BLD 2	Requirement BLD 3 (M1 Zone)	Proposed BLD 3	Target Requirement	Total Proposed
N/A	Warehouse Building GFA - Approx	N/A	104,660 m <sup>2</sup>	N/A	59,053 m <sup>2</sup>	N/A	18,402 m <sup>2</sup>		
N/A	Office Space GFA (5% of warehouse)	N/A	5,233 m <sup>2</sup>	N/A	2,953 m <sup>2</sup>	N/A	920 m <sup>2</sup>		
Zoning By-law 016-2014	For the first 1,000m <sup>2</sup> : 1 parking space per 30m <sup>2</sup> of gross floor area For gross floor area between 1,001m <sup>2</sup> to 5,000m <sup>2</sup> : 1 parking space per 100 m <sup>2</sup> of gross floor area Gross floor area greater than 5,000m <sup>2</sup> : 1 parking space of 200m <sup>2</sup> of gross floor area. (As per section 5.2, 10% deducted from GFA)	520	469	315	296	131	132	966	897
	Number of Accessible Parking Spaces	13	16	9	12	5	6	27	34
	Bicycle Parking 3% of the required parking spaces Loading Spaces 3 Loading space +1 additional loading space for each additional 9,300 m <sup>2</sup> or fraction thereof in excess of 7,441 m <sup>2</sup>	16	18	9	12	4	4	28	34
		14	155	9	109	5	51	28	315

OVERALL SITE PLAN SCALE: 1:2000

GENERAL NOTES

- PROPERTY LINE
- 2.75m x 5.8m PARKING STALL, PAINTED PARKING STRIPING PER CITY STANDARDS. TO HAVE TYPE A STALLS (3400x5800).
- PRINCIPAL ENTRY - TENANT FIT-UP SUBJECT TO INTERIOR ALTERATION PERMIT
- TYPICAL SHARED ACCESSIBLE PARKING STALLS, PAINTED PARKING STRIPING PER CITY STANDARDS. TO HAVE TYPE A STALLS (3400x5800), TYPE B (2750x5800), OR ONE OF EACH WITH 1500mm PATH STRIP BETWEEN - REFER TO CITY OF MILTON'S ACCESSIBLE PARKING STANDARDS.
- 150mm WIDE CURB TYPICAL
- MIN. 1500mm WIDE SIDEWALK TYPICAL U.N.O
- TRAILER PARKING STALL - 12'-0" X 55'-0"
- ACCESSIBLE CURB RAMP AS PER DETAIL
- FIRE DEPARTMENT CONNECTION / SIAMISE
- PROPOSED LOCATION OF TRANSFORMER C/W CONCRETE PAD 1.8m HIGH BLACK VINYL CHAIN LINK FENCING OR APPROVED EQUAL ALONG DEVELOPMENT LIMIT BOUNDARY CONCRETE APRON
- LANDSCAPE AREA - SEE LANDSCAPE DWGS.
- PEDESTRIAN RAIL (1070mm HIGH) SET INTO RETAINING WALL WHERE GRADE CHANGE GREATER THAN 600mm. PROVIDE CONCRETE-FILLED STEEL BOLLARD AT END OF RETAINING WALL - SEE CIVIL DWGS. EXTERIOR STEEL STAIRS W/ TUBE STEEL GUARDRAIL, TYP.
- TRUCK LOADING DOCK (TYPICAL)
- LOADING SPACE - L.S. (MIN. 12.0m X 3.5m)
- FIRE ACCESS ROUTE W/ 12M TURNING RADIUS
- PROPOSED ELECTRICAL ROOM
- PROPOSED MECHANICAL ROOM
- CURB RAIL AT ENTRANCES WITHIN MUNICIPAL SIDEWALK LIMITS TO CONFORM TO OPSD 350.010. - SEE CIVIL DWGS.
- 1.8M WIDE PAINTED PEDESTRIAN PATHWAY
- HATCHED AREA DENOTES HEAVY DUTY ASPHALT. TYPICAL FOR ALL AREAS REQUIRING FIRE TRUCK OR TRACTOR TRUCK ACCESS.
- 15.0m CENTERLINE RADIUS DISTANCE TO FIRE ACCESS ROAD
- ROAD CURB AND SIDEWALK TO BE CONTINUOUS THROUGH THE DRIVEWAY. DRIVEWAY GRADE TO BE COMPATIBLE WITH EXIST. SIDEWALK AND A CURB DEPRESSION WILL BE PROVIDED FOR AT EACH ENTRANCE.
- INVERTED U-SHAPE GALVANIZED BICYCLE RACKS MIN. 1.8mX0.6M PER SPACE
- PROPOSED STOP SIGN LOCATION
- PRESSED PATTERNED ASPHALT PEDESTRIAN PATHWAY
- YELLOW PAINTED LINES
- RETAINING WALL
- PRECAST SCREEN WALL TO BE INSTALLED ON TOP OF RETAINING WALL - REFER TO STRUC. DWGS
- PROPOSED FIRE ROUTE SIGN LOCATION
- RESERVED
- PROPOSED AMENITY AREA
- SNOW STORAGE ON SITE AT 2% TOTAL SITE AREA
- PROPOSED CHAIN-LINK FENCE
- CONCRETE/STEEL SAFETY BOLLARD
- SCREEN WALL
- PROPOSED PYLON SIGNAGE
- DRIVE-IN RAMP WITH GALVANIZED GUARDRAIL ON EACH SIDE. SEE CIVIL DWGS FOR SLOPE %
- PROPOSED NOISE WALL
- DETECTIBLE TACTILE WARNING SURFACE, CONFORMING TO 2012 O.B.C.
- MIN. 3m WIDE CONCRETE DOLLY PAD AT TRAILER STALLS
- ACCESSIBLE PARKING GRADE SLOPING UP TO MEET PROPOSED CURB LEVEL
- FUTURE SHIPPING OFFICE
- WASTE COLLECTION STAGING AREA. TO BE USED TO TEMPORARILY PLACE BINS FOR GARBAGE COLLECTION

SITE LEGEND



**WARE MALCOMB**  
 ARCHITECTURE  
 CIVIL ENGINEERING  
 PLANNING  
 BRANDING  
 INTERIORS  
 BUILDING MEASUREMENT  
 6220 Highway 7, Suite 300  
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 P 905.760.1221

anatolia  
 TILE + STONE

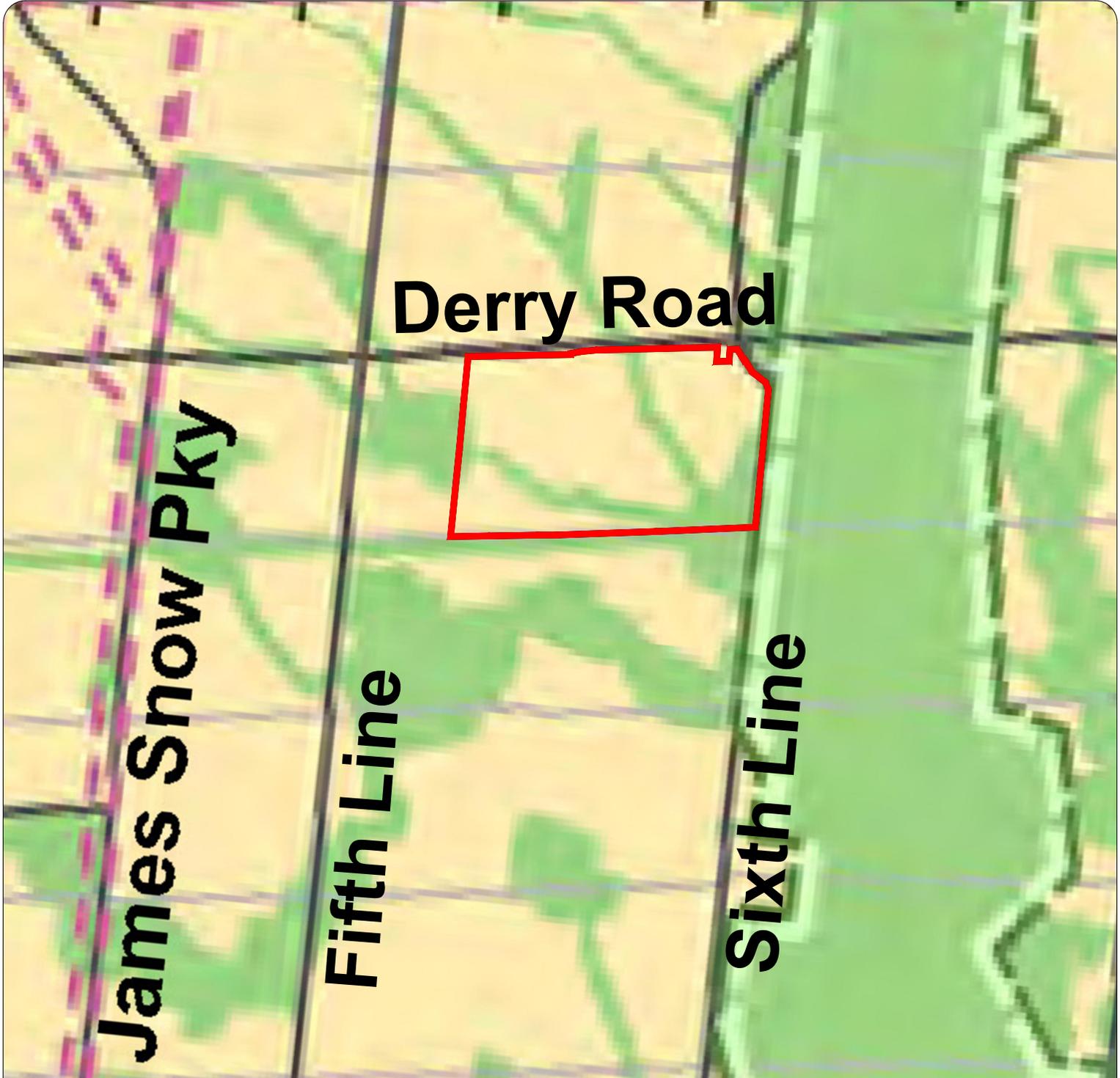
**6728 SIXTH LINE,  
 MILTON**  
 6728 SIXTH LINE  
 MILTON, ONTARIO L9T 2Y3

OVERALL SITE PLAN

DATE	ISSUED FOR	REMARKS
2023-12-15	FOR SPA	

PA / PM: C.R.  
 DRAWN BY: HW  
 JOB NO.: TOR21-0016-01

SHEET  
**A1.0**



Waterfront Park (See Map 2)	Urban Area	Greenbelt Plan Protected Countryside Boundary
Rail Line	Hamlet	Niagara Escarpment Plan Boundary
Proposed Major Arterial	Agricultural Area	Parkway Belt West Plan Boundary
Major Road	Regional Natural Heritage System *	Area Eligible for Urban Servicing
Provincial Freeway	Mineral Resource Extraction Area	Halton Waste Management Site
Lot and Concession Line	North Aldershot Policy Area	Regional Urban Boundary
Municipal Boundary	Greenbelt Natural Heritage System (Overlay)	Deferred Area **

**FIGURE 4** REGION OF HALTON OFFICIAL PLAN  
**MAP 1 - REGIONAL STRUCTURE**

6728 SIXTH LINE  
 PART OF LOT 10, CONCESSION 6  
 TOWN OF MILTON,  
 REGIONAL MUNICIPALITY OF HALTON

SCALE NTS  
 JANUARY 12, 2024

**LEGEND**  
 Subject Lands

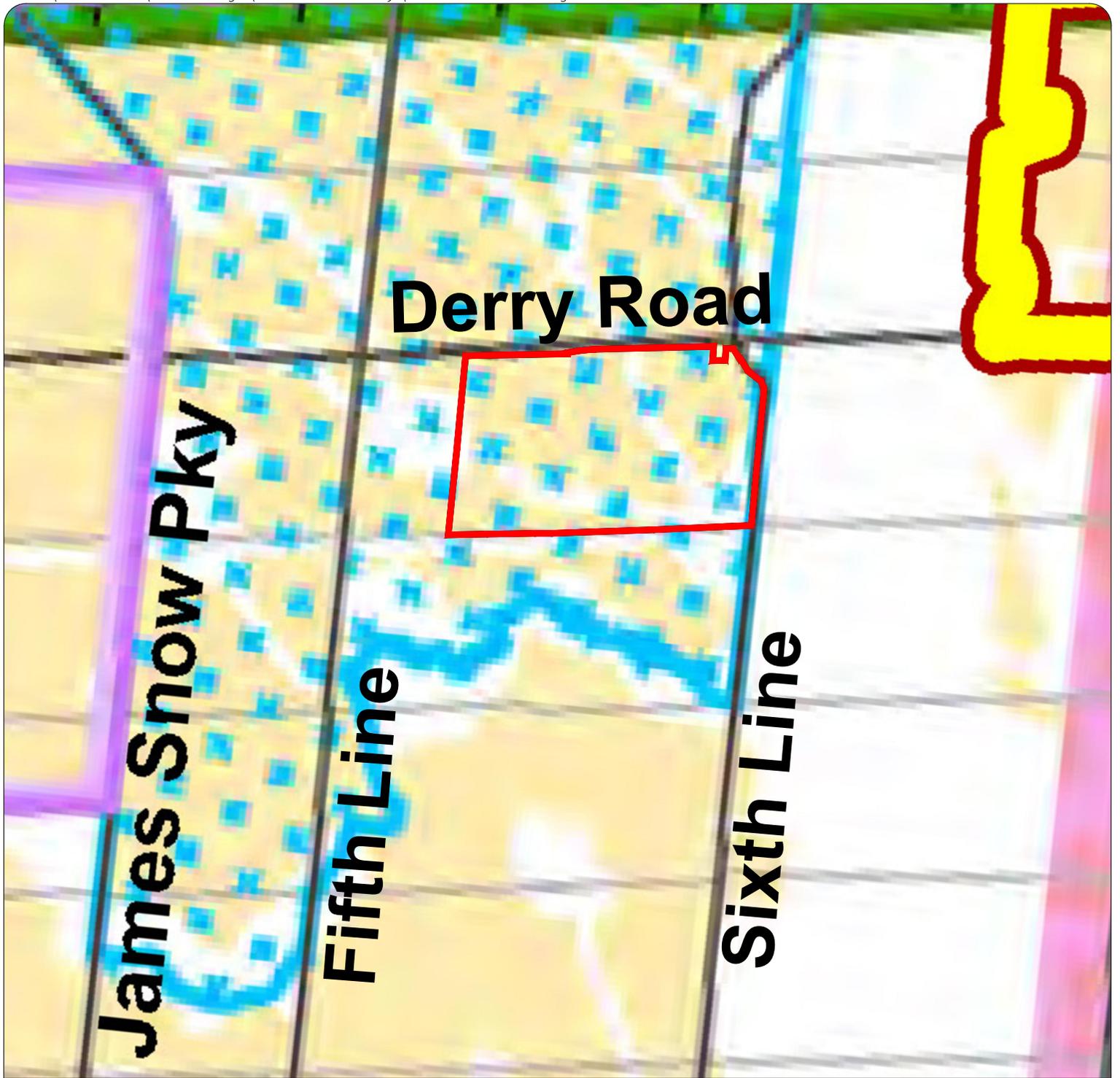


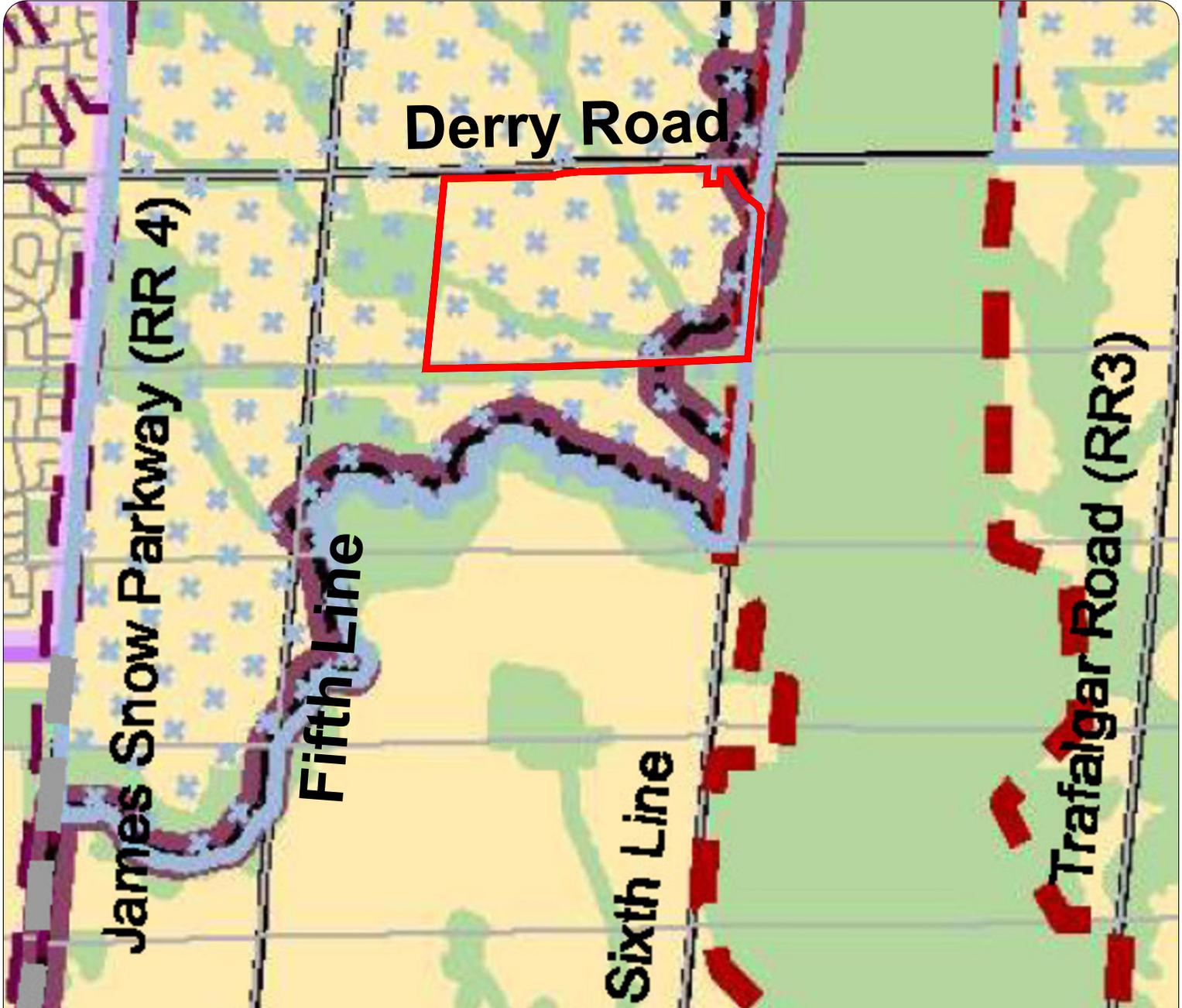
FIGURE 5 REGION OF HALTON OFFICIAL PLAN MAP 1H - REGIONAL URBAN STRUCTURE

6728 SIXTH LINE  
PART OF LOT 10, CONCESSION 6  
TOWN OF MILTON,  
REGIONAL MUNICIPALITY OF HALTON

SCALE NTS  
JANUARY 12, 2024

LEGEND  
 Subject Lands





	Rail		HUSP Urban Area		Proposed Major Transit Station
	Proposed Major Arterial		Agricultural Area		Mobility Hub
	Highway		Mineral Resource Extraction Area		Greenbelt Natural Heritage System
	Major Road		Natural Heritage System		Niagara Escarpment Plan Boundary
	Lot and Concession Line		Urban Area		Parkway Belt West Plan Boundary
	Urban Growth Centre		Hamlet		Greenbelt Plan Protected Countryside Boundary
	Central Business District		Halton Waste Management Site		Municipal Boundary
	SHP Urban Area Boundary		Employment Area		
	Built Boundary				

**FIGURE 6**  
**TOWN OF MILTON OFFICIAL PLAN**  
 SCHEDULE 1 - TOWN STRUCTURE PLAN  
 LEGEND  
 Subject Lands

6728 SIXTH LINE  
 PART OF LOT 10, CONCESSION 6  
 TOWN OF MILTON,  
 REGIONAL MUNICIPALITY OF HALTON

SCALE NTS  
 JANUARY 12, 2024

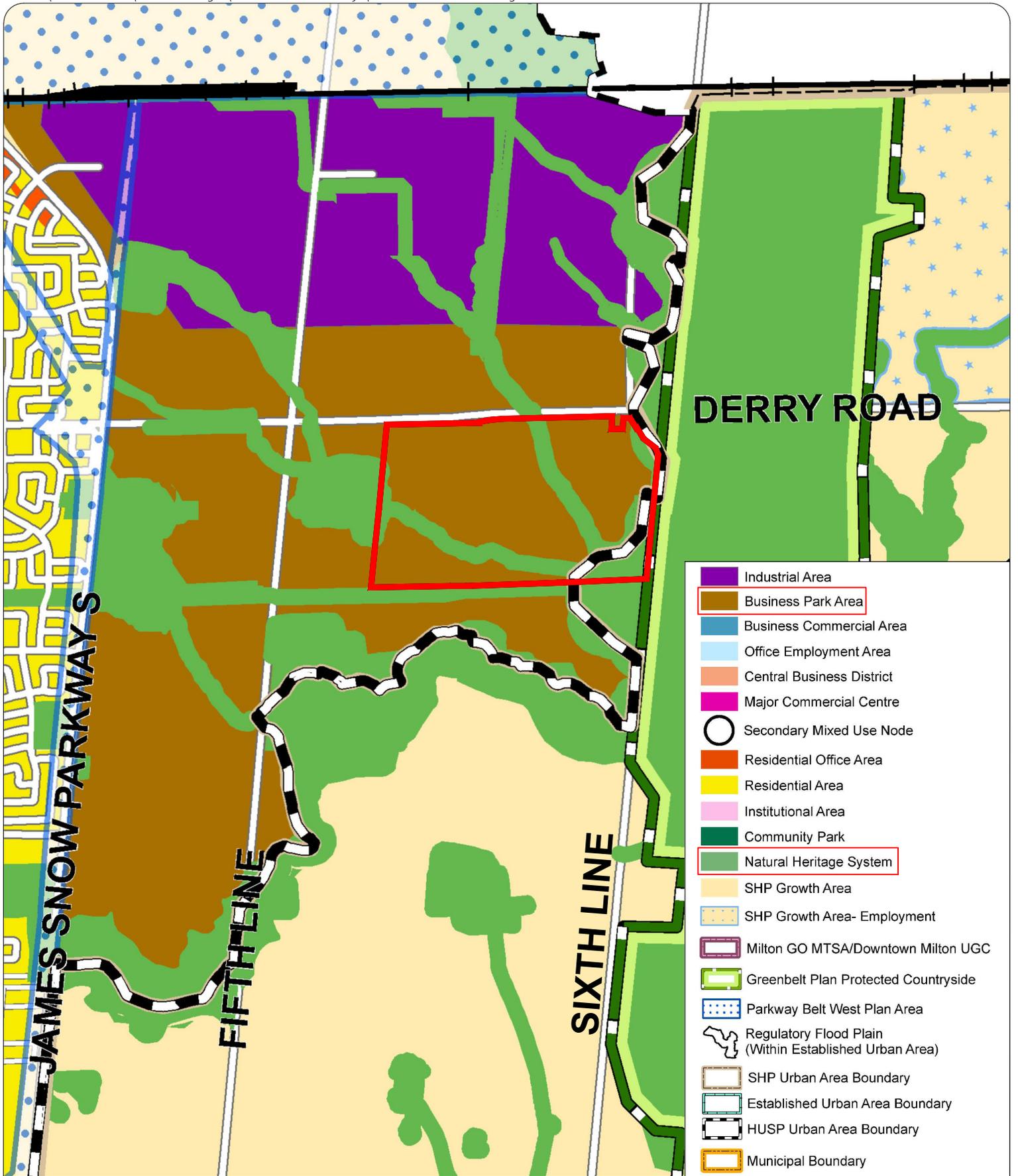


FIGURE 7 TOWN OF MILTON OFFICIAL PLAN SCHEDULE B - URBAN AREA LAND USE PLAN

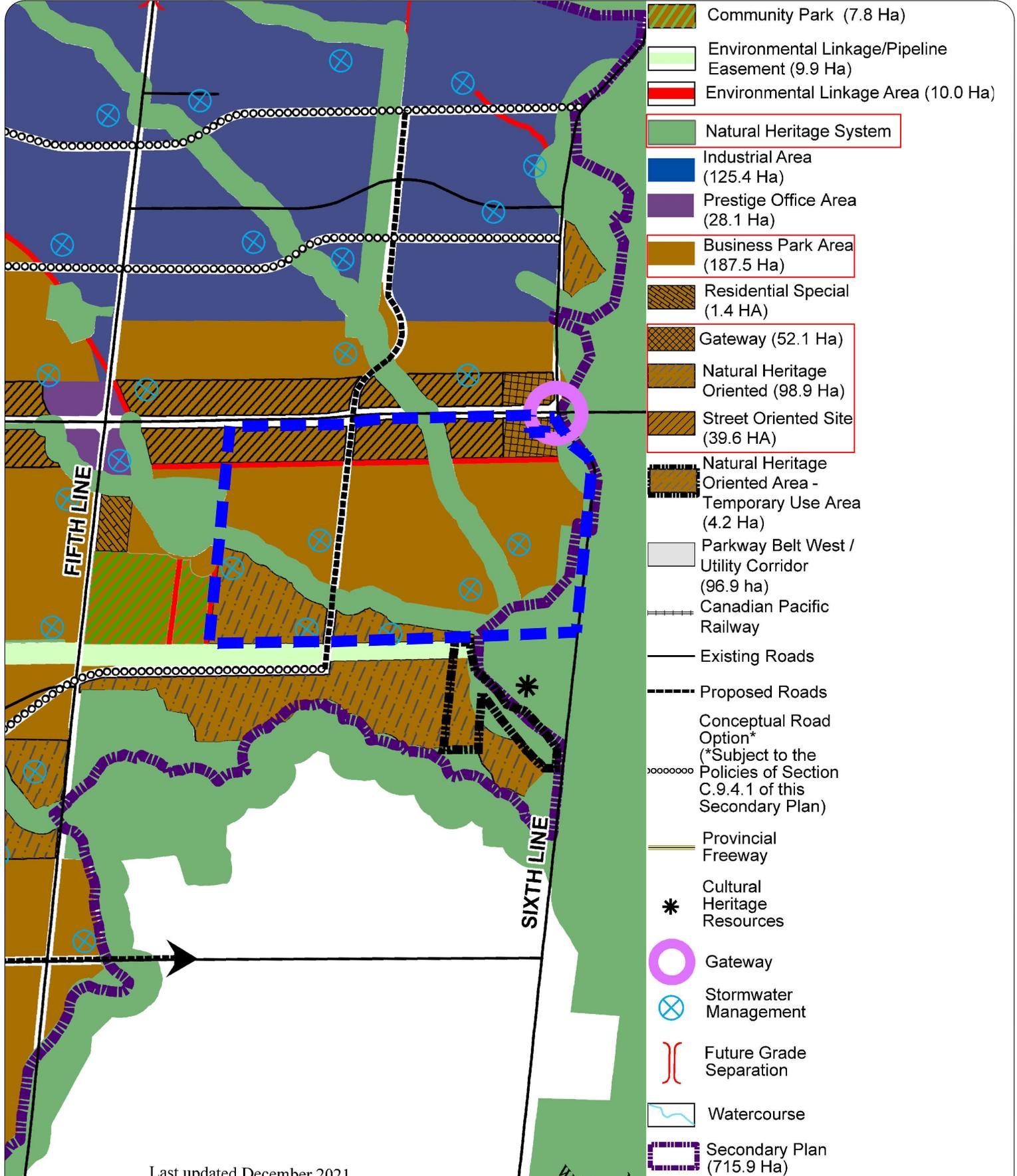
LEGEND

Subject Lands

6728 SIXTH LINE  
PART OF LOT 10, CONCESSION 6  
TOWN OF MILTON,  
REGIONAL MUNICIPALITY OF HALTON



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JANUARY 12, 2024



Last updated December 2021

**FIGURE 8 MILTON SECONDARY PLAN**  
**SCHEDULE C-9-B - DGCBP LAND USE PLAN**

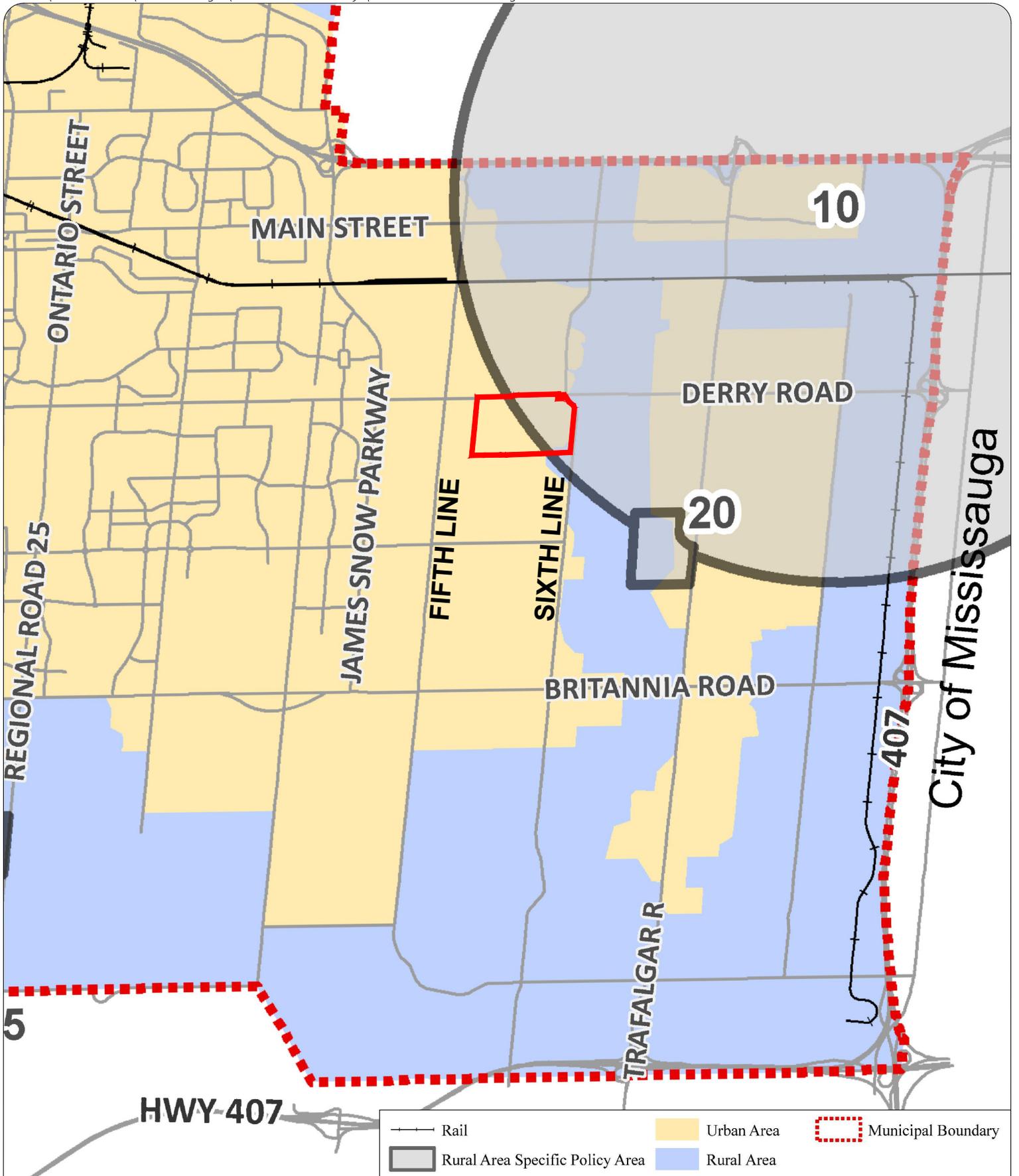
**LEGEND**

Subject Lands

6728 SIXTH LINE  
 PART OF LOT 10, CONCESSION 6  
 TOWN OF MILTON,  
 REGIONAL MUNICIPALITY OF HALTON



SCALE NTS  
 JANUARY 12, 2024



 Rail	 Urban Area	 Municipal Boundary
 Rural Area Specific Policy Area	 Rural Area	

FIGURE 9 MILTON SPECIFIC POLICY AREA

AREA 10

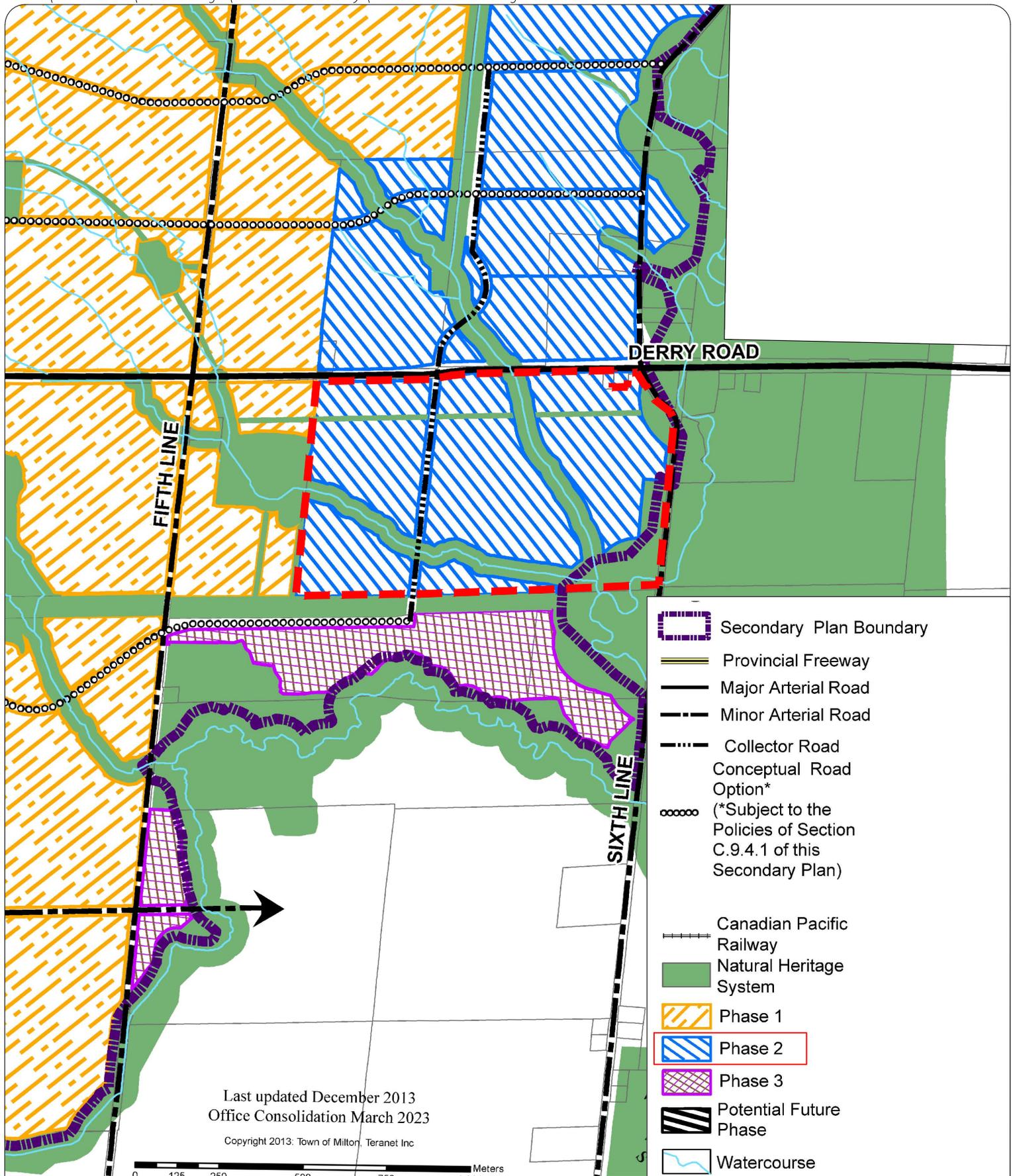
LEGEND

 Subject Lands

6728 SIXTH LINE  
PART OF LOT 10, CONCESSION 6  
TOWN OF MILTON,  
REGIONAL MUNICIPALITY OF HALTON



SCALE NTS  
JANUARY 12, 2024



-  Secondary Plan Boundary
-  Provincial Freeway
-  Major Arterial Road
-  Minor Arterial Road
-  Collector Road
-  Conceptual Road Option\*  
(\*Subject to the Policies of Section C.9.4.1 of this Secondary Plan)
-  Canadian Pacific Railway
-  Natural Heritage System
-  Phase 1
-  Phase 2
-  Phase 3
-  Potential Future Phase
-  Watercourse

Last updated December 2013  
 Office Consolidation March 2023  
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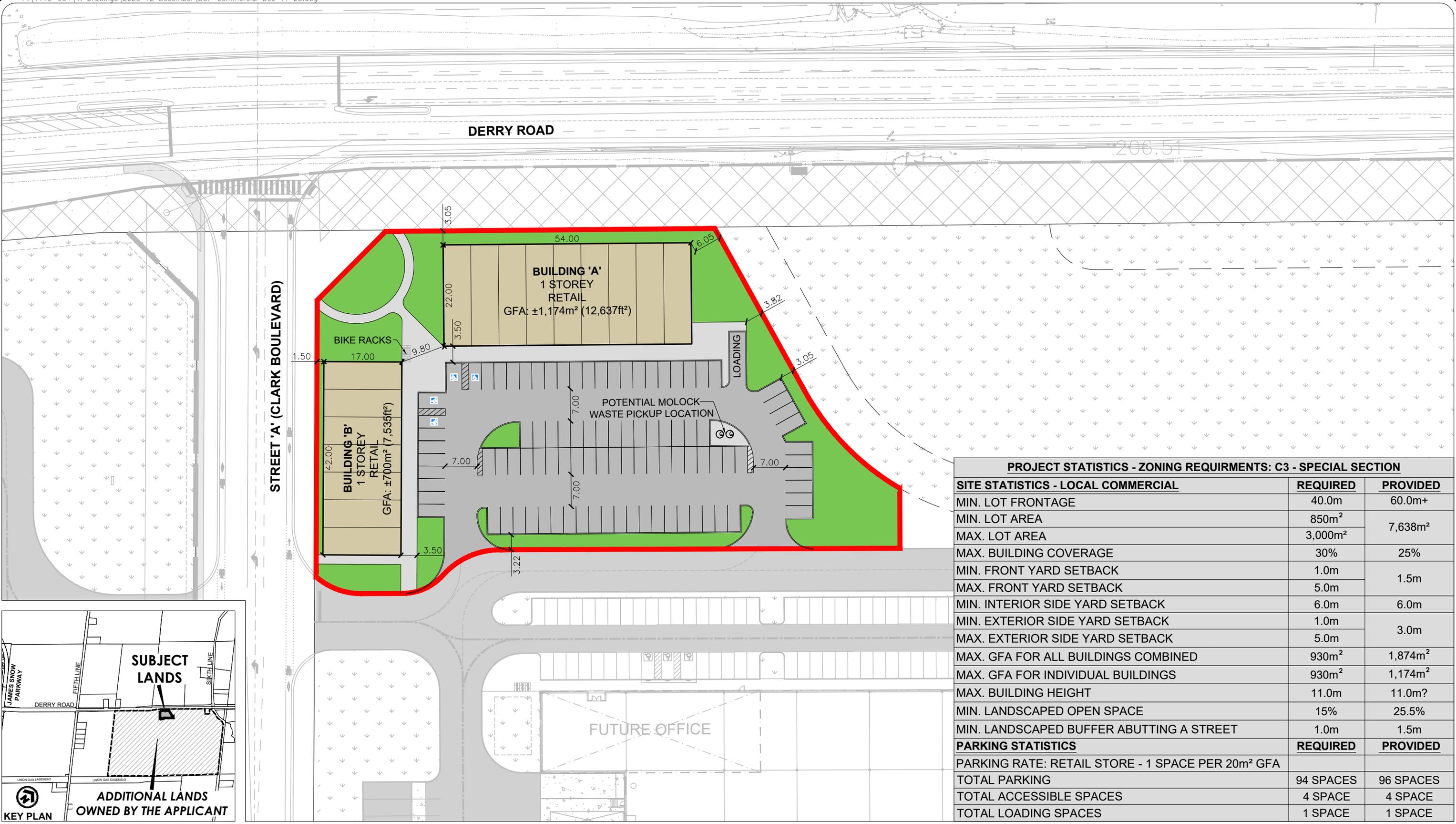
**FIGURE 10 MILTON SECONDARY PLAN**  
**SCHEDULE C-9-C - DGCBP PHASING PLAN**

**LEGEND**  
 Subject Lands

6728 SIXTH LINE  
 PART OF LOT 10, CONCESSION 6  
 TOWN OF MILTON,  
 REGIONAL MUNICIPALITY OF HALTON

 SCALE NTS  
 JANUARY 12, 2024





# SITE CONCEPT PLAN ANATOLIA INVESTMENTS CORP.

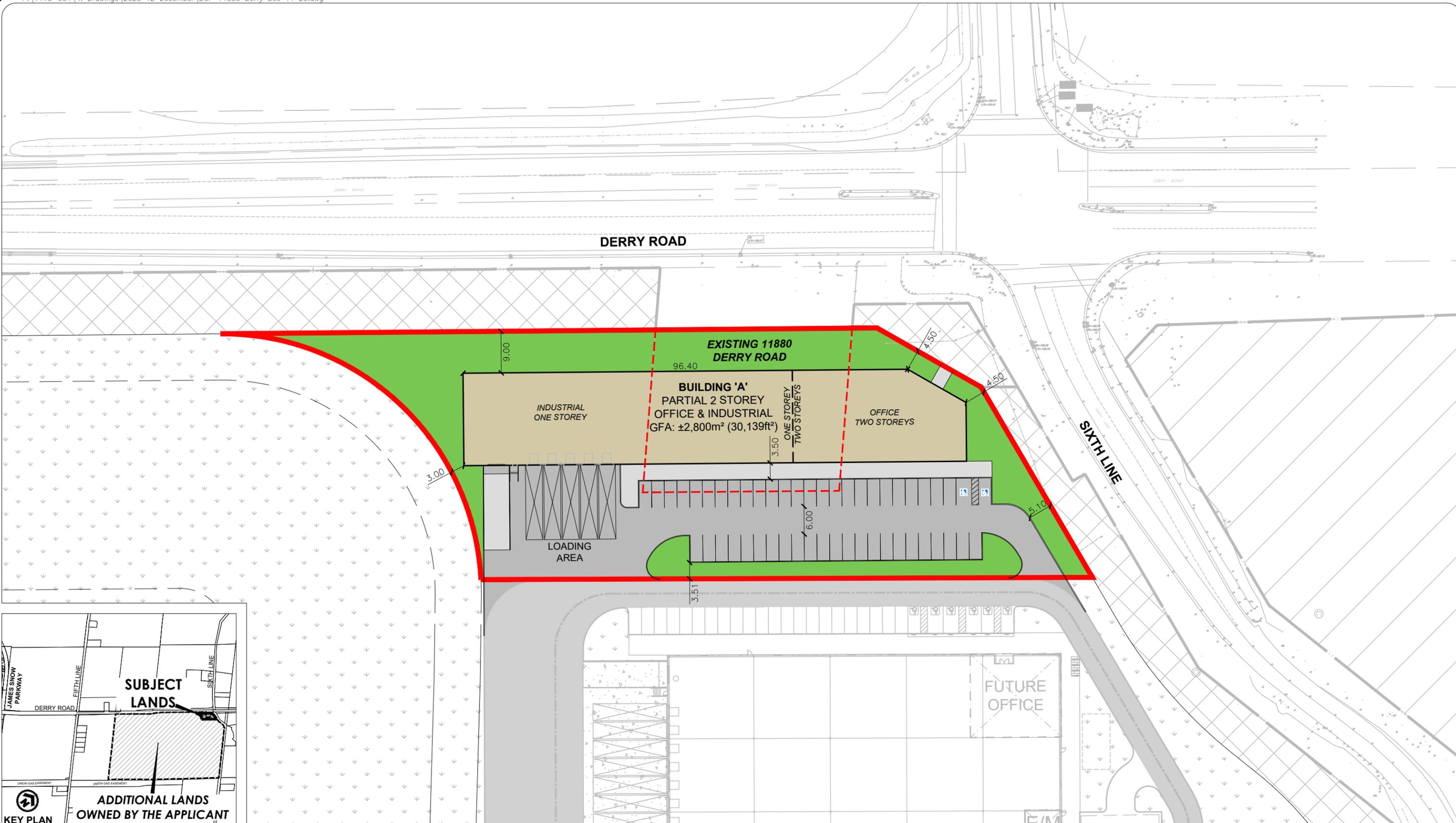
PART OF LOT 10, CONCESSION 6, NEW SURVEY  
TOWN OF MILTON, REGIONAL MUNICIPALITY OF HALTON

FIGURE  
**11**



SCALE 1:750  
DECEMBER 11, 2023





**SITE CONCEPT PLAN**  
**ANATOLIA INVESTMENTS CORP.**

PART OF LOT 10, CONCESSION 6, NEW SURVEY  
 TOWN OF MILTON, REGIONAL MUNICIPALITY OF HALTON

FIGURE  
**12**